



Mad River Byway *EXTENSION*

DRAFT *Nomination Package and
Corridor Management Plan*



submitted to:
**Scenery Preservation Council and
Vermont Agency of Transportation**

submitted by:
Central Vermont Regional Planning Commission

on behalf of the communities of:
Waitsfield, Warren, Granville, Moretown, Fayston and Buel's Gore



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A. About the Vermont Byways Program

The U.S. Congress created the National Scenic Byways Program in 1991, funded under the Intermodal Surface Transportation Efficiency Act (ISTEA), to help “identify, designate, and promote scenic byways and to protect and enhance the recreational, scenic, historic and cultural qualities of the areas through which these byways pass.” The National program gives states freedom to define their own ways of participating in the national effort.

The autonomy provided by the National program has allowed Vermont to create a byways program that meets the needs and circumstances unique to its New England character. Thus, the State – through the Scenery Preservation Council – reestablished a *Vermont Byways Program*, which addresses the state’s need “to make transportation investments to strengthen the economy while also managing and protecting community character.” The State program establishes a process to integrate “growth management, economic development, and transportation investments,” and creates a framework for different interest groups to reach corridor-wide strategies that balance their diverse issues.

The Vermont Byways Program defines a byway as a “highway or other public road that may be associated with other transportation resources that have special scenic, historic, recreational, cultural, archeological and/or natural qualities, and that has been formally designated by the Vermont Transportation Board. A Vermont Byway can be designated and managed with a focus on any or all of these six qualities. A wide range of road types are included: village streets, country lanes, gravel roads in the Green Mountain National Forest, State roads, and Interstate Highways.”

“...the [Vermont Byways] Program gives the state a strong tool to address one part of the always changing challenge relative to how we make our roads safe and efficient for a wide range of users while we still protect the qualities of this state that make Vermont so appealing, its unspoiled rural scenery and farmscapes, picturesque villages, and small historic and human-scale urban centers.”

The Vermont Byways Program Manual 1996, pg. 3

As stated in the Introduction of the *Vermont Byways Program Manual*, roads in Vermont “play an integral role in the state’s economy, its heritage, its character and in the everyday lives of citizens, but they represent a challenging public asset to manage, given that roads must serve the needs of a broad and diverse set of users.” Thus, the Vermont Byways Program

seeks to meet the needs of all users that exist along a roadway, while still maintaining the valuable resources and rural charm that make Vermont such a vibrant and unique place to live.

It is important to note that the development of a Byway is not intended to affect or influence regulatory review processes. As stated in the Byway Manual, the byway is part of a non-regulatory program that creates no new permit processes. A byway may not impinge on or impact private properties or activities. Designation of a Byway means that the Byway community will have access to Federal funds for preservation, programming, management and enhancements, and that is one goal of this effort. It also provides the constituents of the community and the region with an opportunity to create partnerships and initiatives, which facilitate the long-term management and development of the corridor in a manner that is consistent with their vision and goals.

B. About this Document

Participation in the state level Vermont Byways Program requires the submission of a nomination package and corridor management plan, which entails responding to specific criteria outlined in the Byway Designation Application prepared by the Scenery Preservation Council and provided by the State Scenic Byways Coordinator. The nomination forms in Part I of *Designating Vermont Byways: A Fieldguide* were also used to help supplement our response to the criteria and to provide a framework for identifying the six intrinsic qualities.

The criteria required in the Byway Designation Application is addressed in three sections of this document: (1) a brief description of the proposed byway, the level of designation we are seeking and the reasons for seeking it, are provided in this section, *I. Introduction*; (2) the local and regional objectives for seeking designation, the general planning process including public meetings, and vision and theme are provided in section *II. Byway Application Criteria*; and, (3) the reasons why the byway should be designated in terms of the intrinsic resources are provided in section *III. Intrinsic Resources Inventory and Assessment*.

The Vermont Byways Program lays a firm foundation for byway groups that intend to either apply for federal funds or to apply for designation as a National Scenic Byway. As such, this document also serves as a Draft Corridor Management Plan for the Mad River Byway Extension. Upon recommendation by the Scenery Preservation Council and formal

designation by the Transportation Board, this document will become the Final Corridor Management Plan for the Mad River Byway Extension. However, it is anticipated that this plan will continue to evolve and grow with changing conditions, needs and opportunities. This plan is intended to be a “living” document. Thus, it will be revised and refined as the Byway Program for the Mad River Byway Extension is further developed.

C. Mad River Byway Extension Overview

Route 100 is Vermont’s scenic highway, running the length of the state from Massachusetts to Canada. The route follows the Mad River through one of Vermont’s most unique and well-known regions, the Mad River Valley. This exceptionally scenic and resource-rich twenty miles of road is proposed for Vermont Byway designation, from the Granville Gulf Reservation north to the intersection with Route 100B in Moretown. Also included in this designation is a portion of Route 17, from the intersection of Route 100 through the Town of Fayston to the top of the Appalachian Gap in Buel’s Gore.

The valley is a ruggedly beautiful place, carved by the Mad River, framed by the Green Mountains to the west and Northfield Range to the east, rich in history and with a strong agricultural tradition. Views of mountaintops and hillsides, historic villages, rolling meadows, cows, red barns and sugar shacks, all extending from either side of the curving Mad River characterize this route. Route 100 is the backbone of daily life in the Mad River towns, providing access for residents and visitors to take advantage of the Valley’s rich resources. It is also a main transportation thoroughfare, the only north-south route in this remote region, providing access to Waterbury, Montpelier, and the interstate.

The *Transportation Management Plan* prepared by Resource Systems Group describes the corridor in the following manner: “...VT 100 is classified as a rural minor arterial throughout the study area. VT 100B and VT 17 are classified as rural major collectors throughout the study area...As a minor arterial, the official function of VT 100 is to provide for longer trips at higher speeds...VT 17 is classified as a major collector. It should provide an equal balance of mobility for through traffic and access to adjacent land and streets.” (Note: The plan in Appendix A provides greater detail regarding the transportation qualities, conditions, and opportunities existing along the Route 100 and Route 17 corridors). Route 100 is part of the State Highway System and serves as the primary access point to the secondary road network through the region, as well as the

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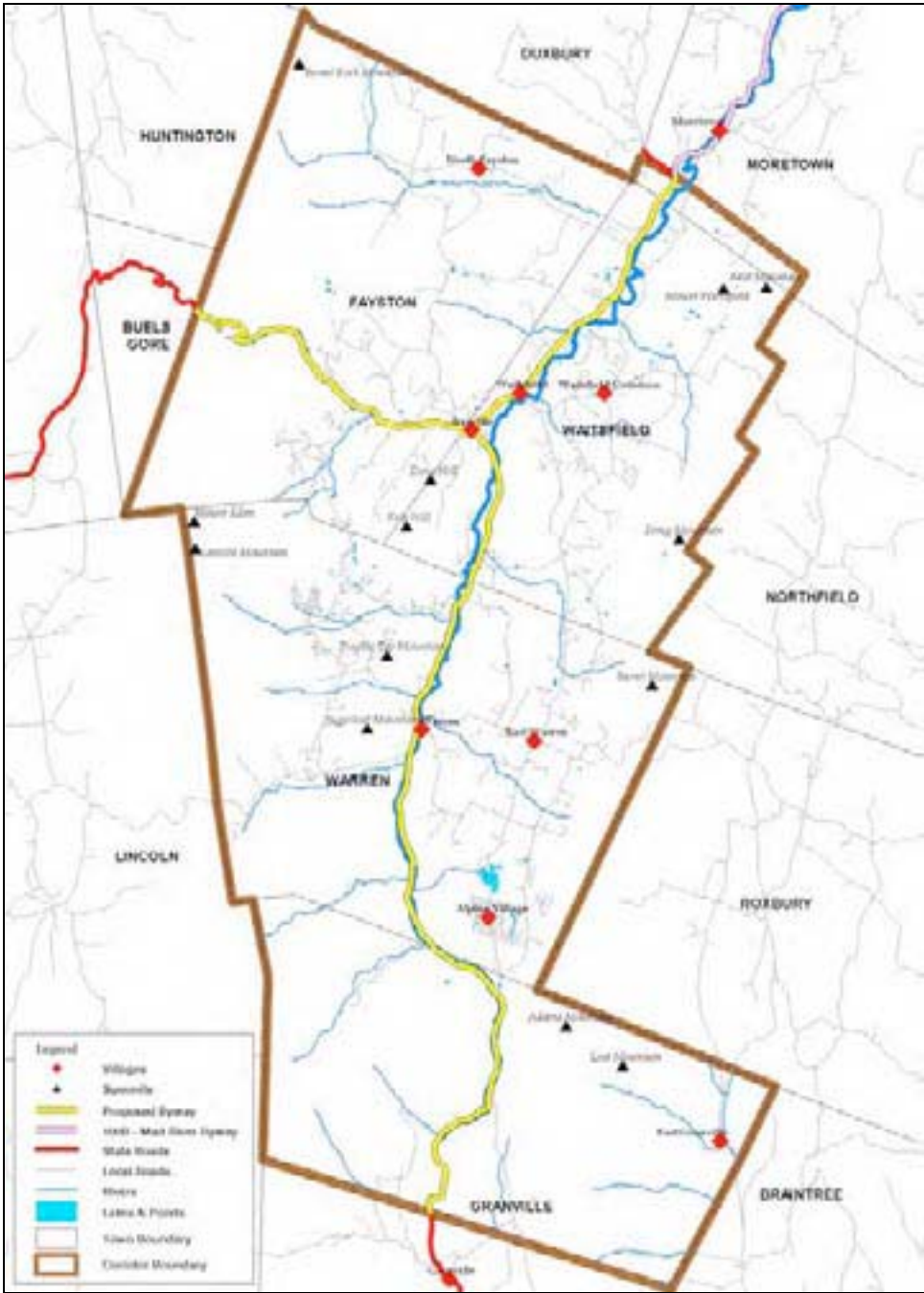
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main commuter route for local residents. The corridor has direct access to and from Interstate 89 in Waterbury, Route 100B in Moretown, Route 17 over the Green Mountains, and the northern and southern parts of Route 100. For the purposes of this application, the width of the corridor has been identified as the town boundaries of Waitsfield, Fayston, Warren, and Granville (to the boundaries of the Granville Gulf Reservation), the section of Moretown from the Waitsfield border to Route 100B, and a small portion of Route 17 in Buel's Gore to the top of the Appalachian Gap (see Map of Byway Corridor below).

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Map of the Byway Corridor



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This section of Route 100 was first surveyed into Warren through the Granville woods as early as 1807, and was known as the Mad River Turnpike. The route was improved in the late 1950's, linking the Valley towns. It is a very scenic route and much of the byway retains its quintessential Vermont character. The establishment of this section of Route 100 as a Vermont Byway will provide initiative and incentive for significant resource management, cultural heritage preservation, and economic development projects in the Valley. To qualify for State and National Byway Programs, the road must exhibit at least one of six intrinsic resource categories: historic, archaeological, cultural, natural, recreational, and scenic. Each of these resources are present in the proposed Route 100 byway, and are confirmed in this report by the Intrinsic Resources Inventory and Assessment and related documentation.

The byway communities along the Route 100 and 17 corridors are Waitsfield, Fayston, Warren, the southwestern corner of Moretown, the Granville Gulf Reservation and Buel's Gore. These communities provide a sampling of Vermont's finest scenic, recreational, cultural, historic, and natural resources. Described below is a brief overview of some of the more notable characteristics unique to each community. For a more detailed listing of resources, please refer to the Intrinsic Resources Inventory and Assessment.

Moretown (from Route 100B intersection to Waitsfield town line)

Moretown was chartered in 1763 by King George the Third through Governor Benning Wentworth of New Hampshire. Even though the town was chartered in 1763, active settlement did not take place until after the Revolutionary War. Moretown is home to a wealth of historic and cultural resources, which provide a view of the region's past and tell the story of human settlement along a natural river corridor. The small section of the town located within the Mad River Byway Extension is part of the Mad River Valley Rural Historic District, representing the strong agricultural tradition of the region. It is also one of the few areas in the Valley where prehistoric evidence of the Native Americans' presence on the land has been found.

Waitsfield

The township of Waitsfield was chartered to Col. Benjamin Wait and others in 1782. The town boundaries included the Mad River and its narrow floodplain, steep hills, and land east of the Northfield Range that is now part of Northfield. Modern development has centered around the river and Route 100 since agricultural endeavors began to decline in the

last century. The town, especially the village of Irasville, is the commercial center of the Mad River Valley, providing services to residents and supporting the ski industry and other tourism-related business. It is also a central location for community events: with playing fields, a skating rink, and many annual festivals and ongoing performances. Even with many modern developments, the town is able to maintain much of its historic character. Waitsfield Village and Waitsfield Common (the town's original settlement location) are nationally listed historic districts. The Joslin Round Barn and the town's covered bridges are nationally listed as well. Historic buildings and barns have been meticulously maintained and the Main Street through Waitsfield Village looks much like it did a hundred years ago. Many farms, since ceasing operations towards the end of the 20th century, have been conserved through various land conservation organizations, maintaining the open meadows and working landscape that defines Waitsfield.

Fayston

Although not bisected by Route 100 like the other byway communities, Fayston depends on the route for north-south access to many services. Chartered in 1782, Fayston is a rugged hill town, reported to have the highest average elevation of any town in Vermont. The town's natural features limited its industrial development to logging, talc mining, and subsistence agriculture in the 1800s. No mill villages were ever developed. With the decline of these industries in the 20th century, Fayston experienced dramatic population reductions. In the years following World War II, Fayston's rugged terrain was recognized as ideal for a new ski resort. Mad River Glen began operating in 1949, and Fayston's population began to increase once again. The McCullough Turnpike, now Route 17, was completed in 1958, crossing the Green Mountains from Fayston into Lincoln and providing an important "gap" and east-west route over the mountains. The Glen Ellen ski area was established in 1962 and is now part of Sugarbush resort. Fayston is mainly a rural residential community, reliant on Moretown and Waitsfield for many daily services, but provides some of the Valley's best recreational and natural resources for visitors and residents alike.

Warren

Warren was chartered in 1789. The natural features of the land have influenced development since the town's beginnings. The first settlement was a small hamlet in the eastern part of the town on level plateaus that provided the best farmland. Agriculture and maple sugaring were the main industries during the town's early years, laying the foundation of

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Warren's working landscape. The mill industry grew around the Mad River in the 1800s, with the development of Warren Village and Route 100. The corridor's other east-west gap road was built in 1817, over Lincoln Mountain. Fires and the flood of 1927 eventually led to the end of the mill industry in the town. Warren Village is a nationally listed historic district and features a unique composition of architectural styles from many different time periods. Two bridges are nationally listed as well, the Warren covered bridge and the Kingsbury iron bridge. Route 100 was redirected around the Village in the 1950s, preserving the historic character of the town's Main Street.

Today, Warren continues to use its large quantity of natural resources for the basis of modern development and to provide extensive recreational opportunities. Much of the town is forested, including 7,180 acres of Green Mountain National Forest and 650 acres of the Breadloaf Wilderness Area. The town's high elevations resulted in the development of the ski industry, which developed slowly at first but now dominates the Valley with its extensive network of related businesses. Sugarbush ski area was developed in 1958. This large resort caused a building boom and development of Sugarbush Village, the center of the region's tourism activity.

Granville Gulf Reservation

The Route 100 corridor extends through the 7-mile long Granville Gulf Reservation. This area was preserved for its scenic beauty and remains untouched and undeveloped. It covers 1200 acres on the eastern edge of the Green Mountain National Forest and contains an old growth stand of red spruce and hemlock trees. The route cuts through the steep slopes of the Green Mountains. Moss Glen Falls is located just off the west side of the road. A small pull-off provides access from Route 100 to a boardwalk overlooking the falls.

Buel's Gore

A small portion of the proposed byway (two-tenths of a mile or 1,056 feet) travels through Buel's Gore along Route 17 to the top of the Appalachian Gap. The triangular shape of the Town can be attributed to a surveying error in which there was leftover land after surveys of surrounding towns were completed. A large portion of Camels Hump State Forest lays within its boundaries and a population of twelve live within this small gore. The Buel's Gore community has raised some concern with regard to increased traffic and traffic safety along this stretch of the highway. This is a circuitous route through the Green Mountains and includes a number of

dangerous switchbacks and sharp curves, as well as steep grades on both sides of the height of land. Appalachian Gap, which at 2,375 feet above sea level, is the divide between the Huntington River and Mill Brook/Clay Brook sub basins in the Winooski River Watershed. The approach up the western slope of the Green Mountains will serve as a gateway to the Mad River Byway Extension that includes Route 17 on the eastern side of the Gap. The Turnpike has a rich cultural history as well as a legacy of accidents due to the hazardous driving conditions, particularly in winter. The management plan forwards specific recommendations to address traffic safety and an understanding of the road's cultural history.

D. Level of Designation

On behalf of the byway communities, the Town of Moretown, Waitsfield, Warren, Fayston, Granville Gulf Reservation, and Buel's Gore, the Central Vermont Regional Planning Commission is submitting this plan for the designation of portions of Route 100 and Route 17 as a "Vermont Byway" under the Vermont Byways Program (Letters of support for this designation may be found in Appendix G).

E. Why We're Seeking Designation

Vermont Routes 100 and 17 meander through the Mad River Valley, offering one of Vermont's most classic New England landscapes. Spectacular mountain ridgelines, winding rivers, hillside farms, covered bridges, steeped villages and an abundance of recreational attractions entice a wide array of visitors and appeal to the many residents who call the Mad River Valley their home. The byway also provides an important link in Vermont's Highway System, transporting tourists, residents and commercial traffic to and from major roads (e.g. I89, US Route 4) and the Mad River Valley.

The proposal to designate portions of Routes 100 and 17 as a Vermont Byway is in recognition of these values and seeks to establish a coordinated plan to enhance and promote the highway corridor and the many resources and attractions found along it. If designated, funds may be available through the National Scenic Byway Program to facilitate planning activities that will support initiatives along

"The Valley is a magical place and I now recognize what a lucky individual I am to have the privilege of growing up here. I would like to come back in 20 years and have my children see it the way I see it now ~ beautiful."

Emily Norton, Warren, age 14; from *Kicking Stones Down a Dirt Road*, page 15

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the corridor, including transportation, preservation, and economic development. This designation recognizes the value and potential of Routes 100 and 17 as a Vermont Byway.

By designating the Mad River Byway Extension, it is also anticipated that distinct benefits will accrue to the byway communities and the region:

- It will enable byway communities, organizations, and regional entities to access federal transportation funds to assist with improvements related to either tourism or resource conservation, such as road safety improvements, pedestrian and bicycle facilities and/or improvements, or protection of natural and cultural resources along the road.
- It will provide a framework for local and regional collaboration between the public and private sector to address common issues. For example, towns can work together to create management strategies to protect special resources and promote community development.
- It will set in motion regional initiatives and plans that are already under consideration for transportation, environmental planning and economic development.
- It will serve as a promotional tool at the local, state and national level and will improve the efficiency of regional marketing and management.

Furthermore, the Vermont Byways Program provides a formal way for the byway communities to identify, protect, and promote the special qualities that characterize the Mad River Byway Extension corridor. It is anticipated that by pursuing the comprehensive “Vermont Byway” designation, initiatives may be undertaken that will improve the transportation system and infrastructure (including pedestrian and bicycle facilities); preserve important natural, cultural, and historic resources; recreational opportunities will become more accessible and more widely used; and, Federal and State funds will become available, ensuring the success of the Mad River Byway Extension as a Vermont Byway.

The establishment of a Vermont Byway following portions of Routes 100 and 17 will represent a significant resource management, cultural heritage, and economic development initiative for Central Vermont, and is a natural candidate for byway designation under the Vermont Byways Program.

A. Development of the Byway Steering Committee

In the spring of 2005, the Mad River Valley Planning District requested the Central Vermont Regional Planning Commission to fund a corridor management plan to extend the Mad River Byway south from VT 100B in Moretown. This was approved, and a consultant was selected in the fall. An introductory meeting was held in October 2005, and numerous meetings and site visits were held, until a formal request to designate VT 100 and VT 17 was approved at the particular Select Boards in August 2007.

The Byway Steering Committee:

Mad River Valley Planning District (which consists of the following)

- Waitsfield Select Board
- Warren Select Board
- Fayston Select Board
- Waitsfield Planning Commission
- Warren Planning Commission
- Fayston Planning Commission
- Mad River Valley Chamber of Commerce
- Sugarbush Ski Resort

Additional organizations that have contributed are:

- Mad River Path Association
- Moretown Select Board
- Moretown Planning Commission
- Waitsfield Conservation Commission
- Warren Conservation Commission
- Buel's Gore Supervisor and Community
- Friends of the Mad River
- Central Vermont Regional Planning Commission
- Vermont Agency of Transportation
- Vermont Agency of Natural Resources
- Green Mountain National Forest

Once designated, the Mad River Valley Planning District will continue the oversight of the Byway Management, and will call upon these and other organizations as needed.

B. Public Planning Process

Public involvement was an important part of developing the Nomination Package and Corridor Management Plan (See Appendix B for Public Meeting Posters and handouts). A number of people gathered for the first formal public meeting that was held on February 9, 2006 at the Yestermorrow School in Warren. The evening's agenda included an introduction and project overview presented by Steve Gladczuk of the Central Vermont Regional Planning Commission, and the following presentations: overview of the Vermont Byways Program by VTrans Scenic Byways Coordinator John LaBarge; Route 100 transportation assessment by Joe Segale of Resource Systems Group; and a summary of work to date, including the intrinsic resources analysis, by David Raphael, Principal of LandWorks. Preliminary inventory boards were presented that illustrated the Mad River Byway Extension's variety of resources, as well as GIS maps. The participants then convened in small groups to discuss a number of prepared questions that addressed two groups: the property owners along the road and corridor, and the users of the road-whether recreational, tourism-related, or business-related. Important elements of the byway concept, vision, and plan were generated at this meeting, and included:

- A focus on the Mad River Path/Greenway as an important byway resource
- The support of byway villages in fostering a sense of community and maintaining the character of the valley.
- Improved sidewalks through civic centers to provide better access to cultural and historic resources.
- Slowing cars in civic places. Stop, look, appreciate.
- Improved access to resources: information and coordination, signs and parking. And preservation of existing important resources.
- Wayfinding
- Redevelopment of the Top Gas property as a central place for tourists to get information
- Bicycle access: connecting to E. Warren Road and other routes. Improve RT 100 for bikes north of Waitsfield Village- make resources consistent

In early March 2006, a promotional handout was created by LandWorks and distributed to the public (See Appendix C). The brochure provided an overview of the proposal for Vermont Byway designation, a sampling of the resources, regional objectives for the byway, and who to contact for

comments and questions. The brochure also announced a second public meeting, which was held on March 23, 2006. The purpose of this meeting was to confirm, refine and further develop goals/objectives/strategies for the byway corridor and the Corridor Management Plan. Many creative ideas, comments and concerns were generated at this meeting and used to finalize the byway goals and objectives. A final publicly warned meeting was then held on July 26, 2007 to present the Corridor Management Plan and receive final comment and consensus (see Appendix D for meeting notes, handouts, posters, sign-in sheets and newspaper articles).

C. Vision and Theme

The vision and theme for a Byway evolves from the work of the Steering Committee and community participants and provides a focus and a “raison d’être” (reason for being) for the Byway as a whole. The theme or themes provide a sense of purpose, which guides the development of the Byway on the ground and the programs or initiatives to be associated with it.

The Vision for the Route 100/Route 17 Byway proposal can perhaps be best articulated in this fashion: The “Mad River Byway: A corridor through the valley community linking people and place.”

Indeed, the themes that emerged again and again in the community visioning sessions and from the various constituencies involved reinforced the value of community character, the conservation of the landscape and the preservation of village life. It is envisioned that the Byway can promote and integrate the wide range of outstanding scenic, cultural, recreational and historic resources. The previous designation of Route 100B in Moretown and Middlesex as an official Vermont Byway coupled with this endeavor provides the prospect of connecting the entire Mad River Valley to create a unified corridor. In this manner the individual communities can collaborate on common goals and share resources in achieving them. Thus “the whole” will surely be greater than “the sum of its parts.”

The vision and theme, as stated, evolves from the key concepts which emerged from the community gatherings and input and these concepts or themes are organized around 4 primary areas of opportunity:

1. Community Togetherness/Gathering

It is proposed that the Byway is conceived as a way to continue to bring a civic-minded community together. The byway should have the goal of

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linking the 5 communities together with common events that citizens for all of the towns get excited about and participate in. It will be important to ensure that development pertaining to the byway (like the ideas for the Top Gas site), include not just information for visitors but also gathering places for the residents of the byway. History, woven through structures, sites, and stories can also bring the community together and should be highlighted in a manner that helps it to come alive and have relevance for the future.

2. Main Street Character

There is a desire to enhance the sections of 100 (and 100B) that are considered main streets. Options for accomplishing this include gateways, street trees, pedestrian amenities, public art and information systems including signs. These enhancements will attract and engage visitors as well as residents in the daily life of the villages, which serve as the heart of the communities. The quality of village life can be enhanced as well. New development in the villages and along Main Street can be guided in a manner that increases density and yet compliments architectural character and historic settlement patterns.

3. Preservation

The Byway can help celebrate the scenic resources that are present in the valley and support the awareness and protection of these resources. Natural resource conservation can be linked with the built environment in a manner which helps sustain the unique integration of people and nature which has long been a part of the valley's traditions, and its commerce and culture. The presence of historic buildings, agricultural landscapes, covered bridges and pine groves highlight how the valley's residents settled in their landscape over time. The goal is to continue this relationship with the land and its resources so as to promote contextual design and a sustainable development pattern for the future.

4. Traffic and Pedestrian Safety

A main theme for this area of is to continue to work toward a safer, more functional and amenable highway network that promotes safe travel, transportation alternatives and an increase in opportunities for pedestrians and bicyclists.

The Management Plan, with its objectives and activities, will thus be organized around and based on these four goals which will fulfill, for the most part, the vision for the future of the Mad River Byway.

D. Local and Regional Objectives for the Byway

Five important goals are proposed for the Mad River Byway Extension. These local and regional goals provide the basis for identification, implementation, and management of the corridor's intrinsic resources and are intended to stimulate and promote economic, recreational, and environmental benefits for the corridor's communities and transportation systems. Most importantly, the goals supply the foundation for the long-term objectives and policies, which will be developed as part of the Corridor Management Plan. The five goals proposed for the Mad River Byway Extension include:

1. Multi-Modal Transportation

To provide reliable and harmonious multi-modal travel options.

2. Recreation

To improve access to recreational resources and facilitate collaborative approaches to recreational development and management.

3. Rural Character

To preserve and enhance the rural character and community values of the Byway Corridor.

4. Preservation

To plan for the preservation and interpretation of the unique scenic, historic, cultural, and natural resources found within the Byway Corridor.

5. Promotion

To develop effective and integrated marketing and information strategies for Byway users and provide appropriate amenities for the traveling public.

E. Conformance with Local and Regional Planning Documents

The proposed Vermont Byway Designation and associated objectives and enhancements described in this package correspond with the goals and policies outlined in each of the byway communities' planning documents (excerpts from the plans are provided in Appendix E). The town plans provide additional information about the character and function of the roadway as well as listings of significant resources. Brief summaries of the overall content in each of the plans is provided below:

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Granville Town Plan

The Southern gateway to the Mad River Byway Extension begins in the rugged and wild Granville Gulf, a valuable scenic resource to all residents and visitors. The town encourages the protection of the natural environment while enhancing outdoor recreational opportunities and preserving the town's history to maintain the local sense of community.

Warren Town Plan

The designation of Routes 100 and 17 as a Vermont Byway would significantly aid Warren's goals to preserve, promote and protect the town's rural character, cultural heritage and historic working landscape. The designation would also help the town's goals for the maintenance, careful stewardship, preservation and enhancement of Warren's natural resources and environmental quality for future generations. The outstanding scenic and recreational resources, and abundance of historic resources that are accessible by the public makes Warren a significant part of the Mad River Byway Extension. Warren's scenic roads are among the town's most valued assets while cultural and historic resources offer a link to the past and define the town's character. Preservation of the town's historic settlement pattern and the resulting character of Warren Village is a high priority for the town, which the town wishes to accomplish in part by creating opportunities for alternative transportation.

Waitsfield Town Plan

The integration of natural and cultural features create a distinct sense of "place" that is unique to Waitsfield and it is the town's intention to promote the responsible stewardship and sustainable use of Waitsfield's resources. The town's rural character is highly valued and local residents support its preservation, including the traditional settlement pattern, historic built environment, and scenic features. The hills and fields, trees and pasturelands, which dominate the landscape, are valuable scenic and natural resources to the community. The scenic beauty of the town is important to residents' quality of life and the preservation of open space for long distance views is encouraged. Waitsfield Village serves as the commercial, cultural and civic center of the town, and it is important for the town that this continues. The town values the protection and enhancement of recreational resources, especially a connected trail system throughout the valley, and identifies Route 100 north of the Village as a scenic road.

Fayston Town Plan

Fayston is an important part of the Mad River Byway Extension, providing an east-west travel corridor via Route 17. The preservation of historic and archaeological resources is important to the town, including the celebration of Fayston's unique history. Fayston also recognizes the need to preserve and enhance the town's rugged natural environment for future generations. The town would like to see opportunities for transportation alternatives, including a network of pedestrian and bicycle paths linking the Valley towns. Scenic beauty is important to the town, as it contributes greatly to the local economy and quality of life. This scenic beauty provides unique recreational opportunities that the town aims to incorporate with its long-term planning

Moretown Town Plan

The Moretown Town Plan provides information about the town's significant resources. In general, the town believes that scenic, natural, archaeological, recreational, historic and cultural resources are important to the community, and each component is referenced in the plan in some aspect. Opportunities for the preservation and enhancement of these resources are desired. Scenic values are especially important to the town, as is the protection of wildlife habitat, and the establishment of a formal recreational trail system.

Central Vermont Regional Plan

In general, the CVR Plan considers scenic, natural, archaeological, recreational, historic and cultural resources important and worthy of maintenance/preservation/enhancement. Each element is referenced in the plan in some aspect and strategies for their protection are included, which support the Byway nomination. These vary from adaptive use of cultural sites and structures, to supporting public transit, to promoting recreational opportunities, to maintaining existing roadside views by means of vegetation clearing. Scenic values are especially important as expressed in the Plan: "Central Vermont is a place of celebrated natural beauty. Its scenic landscapes not only enrich lives and spirits, and attract new businesses and residents, they also provide the basic ingredient for one of the Region's most important industries - tourism. Each year thousands of visitors travel here to see the mountain vistas, pastoral scenes, fertile valleys, historic villages, Interstate 89 (which has received awards for its scenic corridor), remote back roads, and woodlands ablaze with autumn color. Thus, it is in best interest, both psychologically and economically, to preserve the best of Central Vermont's visual splendor." (page 14)

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Green Mountain National Forest Land and Resource Management Plan

As expected, the Green Mountain National Forest (GMNF) Land and Resource Management Plan seeks to maintain the values for which these lands were preserved – wildlife, wilderness, recreation, scenic beauty, and cultural heritage. In this respect, many of the strategies outlined in the Plan support the Byway nomination. For example, Goal 12 of the Forest-Wide Management Direction Section seeks to “Provide a diverse range of high quality, sustainable recreation opportunities that complement those provided off National Forest System lands” and Goal 15 aims to “Maintain or enhance visual resources such as viewsheds, vistas, overlooks, and special features” (p. 16). While there are no conflicting policies between the GMNF Plan and the Byway Corridor Management Plan, it will be important for the Byway to respect and adhere to GMNF policies that may impact future Byway use and development. For example, Byway users will need to be informed of trail use designations in GMNF lands (e.g. motorized use - allowed vs. prohibited in certain areas; or, restricted access for resource protection). The Byway Committee shall also be responsive to the limitations on the development of roads, recreation facilities and trail signs in identified GMNF areas (i.e. Wilderness, Remote Backcountry, Alpine Ski Areas, etc.).

Conclusion

In conclusion, the proposed Vermont Byway Designation and associated objectives and enhancements described in this package correspond with the goals and policies outlined in the Regional and Town Plans for communities along the Mad River Byway Extension as well as the Green Mountain National Forest (GMNF) Plan. Collectively, the communities and GMNF wish to preserve the scenic quality of the area, wildlife habitat and natural resources, the unique character of each Village, and the rural agricultural surroundings. This area is brimming with various culturally, historically, architecturally, and archaeologically significant locations and events, all of which add to the area’s unique character, something these communities wish to preserve as well. Routes 100 and 17 are significant roadways, providing access into and out of the Mad River Valley. Preserving the historic and economic importance of the road while maintaining a safe road system is a priority of the Byway communities.

Introduction

In order for a road to qualify as a Vermont Byway, it must show at least one of the six intrinsic qualities as outlined by the State Byways Program: archaeological, cultural, historic, natural, recreational and scenic. The Intrinsic Resource Inventory and Assessment was the first step in the process of preparing a nomination package and corridor management plan to designate Routes 100 and 17 in the Mad River Valley as a Vermont Byway. The designation provides a formal way for the community to identify, conserve, and promote the Mad River Byway Extension and its important resources.

Information for this inventory and assessment was drawn from a number of sources. An initial inventory was compiled using information from Town Plans, promotional materials distributed through the Chamber of Commerce, and literature from the Friends of the Mad River, the Mad River Watershed Conservation Partnership, and the Mad River Valley Rural Resource Commission. The primary sources for GIS data were the Vermont Center for Geographic Information (VCGI) and the Mad River Watershed Conservation District. Resources were crosschecked with these sources and the community members involved in the process, and mapped as accurately as possible. Intrinsic resource maps illustrating this data are provided at the end of this section.



The following summaries provide an overview of the resources present in the Mad River Byway Extension. A more detailed catalog of Byway resources can be found following this narrative summary in the *Matrix of Intrinsic Resources*, which inventories all of the resources located throughout the Byway and delineates them by town according to one or more of the six intrinsic categories. This inventory is as comprehensive as possible and is anticipated to continue to be examined, assessed, and refined throughout the ongoing Byway process in order to provide the most accurate representation of the evolving Mad River Byway Extension and its many resources.

Natural Resources

Introduction

The Mad River Valley corridor contains some of the most important natural resources in the area, which are of interest and value to a variety of Byway users. The Mad River and its tributaries, rolling hills and valleys, extensive forested lands, large tracks of undisturbed and protected lands, and some of the state's best agricultural soils, all framed by the Green Mountains and the Northfield Range, provide the basis for the corridor's other intrinsic resources. This variety of resources creates a combination of ecosystems important to humans and wildlife and exemplifies the Valley's unique character and quality of life, enjoyed by residents and visitors alike.

Water

The Byway corridor's most prominent natural resource is the Mad River, which meanders and follows this section of Route 100. It was important historically, providing power during the region's mill era, transportation, and a central place of settlement. The Mad River floodplains provide prime agricultural soils that are still in use today. The river is also the foundation of much of the corridor's recreational and scenic resources, offering boating, fishing access, ideal locations for trail systems, and some of the state's finest swimming holes. These resources are maintained for public use and are an important tourism draw. In recognition of the



importance of the Mad River resources, The Friends of the Mad River was founded in 1990 to actively protect and improve all qualities of the river and its watershed. The Mad River also provides habitat for trout populations and other wildlife.

The Mad River's tributaries feature additional swimming holes and scenic waterfalls and trails, as well as beaver ponds and deer wintering habitat. Blueberry Lake in Warren is a significant water source, as the area's only substantial lake and wetland area. Many small wetlands also provide important habitat for wildlife, including palustrine (upland) wetlands in Fayston. These wetlands are formed on glacial tills at high elevations and can retain and process large amounts of surface runoff.

Mountains and Forested Land

The Route 100 part of the Mad River Byway Extension runs through the Mad River Valley, bounded by the Green Mountains to the west and the Northfield Range to the east. These extensively forested mountains play a major role in the ecological, economic, and social health of the regions. The mountains serve as the backdrop to the Byway's scenic views; they provide some of the most productive black bear habitat in the state; they support recreation and tourism activities like hiking, hunting, and skiing; and they are the backbone to the area's largest employer, Sugarbush Resort. Much of the corridor is forest covered, indicating low-density development and minimum human disturbance. Public access to the forests are maintained by the Green Mountain National Forest, as well as the Camel's Hump State Forest, Camel's Hump State Park, the Granville Gulf Reservation, the Scragg Mountain Municipal Forest, and the Phen Basin Wilderness Area.

Sensitive Areas and Wildlife

The Mad River Byway Extension corridor has fragile areas of ecological value that warrant special attention. These areas include but are not limited to: wetlands, floodplains, steep slopes, rare and endangered species, and sensitive riparian vegetation. Due to the steep nature of the river streambanks throughout valley, floodplains are limited. However small, they serve as important "safety-valves" by retaining runoff and floodwaters, and limit development due to this periodic flooding. Floodplains also improve water quality by serving as settling areas for contaminants in stormwater prior to reaching the river. Large areas of steep slopes include areas at high elevations as well as locations along the Mad River's banks. These areas are prone to erosion and are a major component of the corridor's scenic quality. In Warren, the Vermont Non-Game and Natural Heritage Program, a division of the Fish and Wildlife Department, identified two rare plant communities in the high elevations of the Green Mountain National Forest, as well as a rare species of orchid, *Listeria auriculata*, in the vicinity of Blueberry Lake. Audubon Vermont has identified important habitat in Fayston for one state-listed species of "special concern," the Bicknell's Thrush.



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The corridor is also home to extensive deer wintering areas identified by the Vermont Department of Fish and Wildlife. These are found on south or west facing slopes with coniferous forests, and generally at elevations below 2,000 feet. These deeryards provide necessary cover and browse during the winter months. In Fayston, deeryards are limited to the lower Shepard and Mill Brooks. Waitsfield contains approximately 4,000 acres



of deeryards, concentrated primarily along both sides of the Mad River in the undeveloped areas, and in the Folsom Brook drainage. In Warren, deeryards are concentrated along the steep valley wall separating the river valley from East Warren, on West Hill and the surrounding area, and along both sides of Lincoln Brook. The Granville

section has deeryards along the eastern bank of the Mad River. Because deeryards vary from year to year, these locations must continually be re-examined and updated.

Black bear habitat is another important natural resource of the Mad River Byway Extension, as well as being some of the best in the state. These areas include stands of hard mast tree species (mainly beech and oak), which are critical feeding habitats and sources of nutrition in the fall and spring. The quality of this habitat directly affects black bear survival and cub production. In Waitsfield, black bear habitat is found in the upland areas of the Northfield Range and in the vicinity of Camel's Hump State Park. In Fayston, nearly the entire town has been identified as black bear habitat by the Vermont Department of Fish and Wildlife, including



important high elevation parcels held by Sugarbush and Mad River Glen. And in Warren, the Slide Brook basin, located between Lincoln Peak and Mount Ellen in Fayston, has been identified as some of the most productive bear habitat in the state, with the largest and most intensively used beech

stand in the state. Warren also contains significant black bear habitat in the Northfield Range, which has been threatened by increasing

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development in that area. Additionally, black bear habitat is located within the entire section of Granville in the proposed Byway designation. Much of the habitat in the corridor is located on conserved land or on lands subject to Act 250 permit conditions. This is fortunate, however some degree of uncertainty exists in the permitting process and additional protection would be beneficial.

Potential wildlife crossings of major roads have also been identified and correspond to the known deeryards and black bear habitat. There are 16 potential crossing sites in Waitsfield with five (5) on Route 100, over 30 each in Fayston and in Warren, 3 on Warren's section of Route 100, and 2 crossings in the small Granville part of the Mad River Byway Extension corridor. These crossings should be considered and appropriately buffered from development of the roadway.

Agricultural Land

Agriculture is a vital part of the Mad River Valley's rural heritage and an industry strongly based on natural resources. This working landscape is crucial to maintaining rural character, and provides aesthetic value to residents and visitors. Although the number of working farms in the area has declined, several are still in operation, taking advantage of the corridor's most productive agricultural soils in the Mad River floodplains. Agricultural land is important to the scenic character of the area with grazing herds keeping land open and views unobstructed, as well as providing recreational opportunities and riparian corridors for wildlife along the Mad River and tributaries. Many historic farms and surrounding lands have been conserved, protecting prime secluded wildlife habitat and deeryards, while also promoting economic interests through sustainable harvesting of wood resources.



Geologic Resources

The Byway also contains an abundance of geologic resources. Historically, talc mines operated in the area. There are also records of marble and iron deposits that were mined at one point. Sand and gravel deposits are present as well, and have been extracted in Waitsfield and Warren over the years. Gravel extraction today is much less than in the past due to the

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decreasing resource base and strict regulations on the environmental, unsound practice of river bed gravel extraction. Recreational collectors can pan placer gold from the Mad River, a unique recreational quality of the Byway. Significant visual features include the natural bridge in Warren - one of two known arches in Vermont, Moss Glen Falls in the Granville Gulf, several other waterfalls, and many rock outcrops along the Byway. These resources contribute not only to the natural resource base but to the scenic and unique beauty of the area as well.



Recreational Resources

Introduction

This section of the Byway features an abundance of recreational opportunities, providing a major backbone to local economies as well as defining the character of the Mad River Valley. With two major downhill ski areas, national and state forest lands, and miles of trails for cross country skiing, snowshoeing and snowmobiling located within the Byway towns, winter recreation is one of the Valley's greatest assets. In the summer, recreation turns to hiking, swimming, and boating opportunities in and around the Mad River and surrounding mountains. The Agency of Natural Resources has identified the Mad River as a significant recreation resource of statewide importance. There are many publicly owned recreational areas as well as numerous private ventures that contribute to the Valley's culture.



Winter Recreation

Winter recreation is one of the Mad River Byway Extension's defining features, with Routes 100 and 17 providing important access to these resources. Fayston is home to the Mad River Glen ski area and the Mt. Ellen section of Sugarbush Resort, which extends to Lincoln Peak in Warren. Mad River Glen is known for its skiers-only classic New England trails, while Sugarbush's two mountains host a half-pipe, terrain park, and over a hundred trails. Cross country skiing opportunities are found all over the corridor: Warren hosts Ole's Cross Country Center and the Blueberry Lake Cross Country Center, together with over 70 km of groomed trails; The Round Barn Farm Cross Country Ski and Snowshoe Center in Waitsfield offers 25 km of groomed trails with spectacular views of the Green Mountains; Fayston is home to the Catamount Ski Trail, a popular cross country skiing and snowshoeing trail; many Mad River Path Association



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trails and unplowed access roads provide ideal terrain as well. Snowshoe trails are available at many cross-country ski areas as well as Mad River Glen and Sugarbush ski areas, the Phen Basin Wilderness, and along the Long Trail. The Vermont Association of Snow Travelers (VAST) supports snowmobile trails throughout the Mad River Valley corridor with access at “The Pines” Park and Kenyon’s Farm as well as other locations. The Mad River Ridge Runners club maintains the VAST trails.

Ice-skating and rentals are available at Waitsfield’s refrigerated Skatium and many local ponds are cleared for skating as well. Sledding is another popular winter activity enjoyed by children and adults alike. Warren is the home of the Mad River Rocket sled company and the Lincoln Gap Road, which, when closed in the winter, provides the biggest sledding hill around. Horse-drawn sleigh rides are offered at the Mountain Valley Farm in Waitsfield. Numerous inns and restaurants support these winter attractions including Tucker Hill Inn, the first lodging establishment purposefully built to support the area’s ski industry.

Warm Weather Recreation

As the snow and ice melts away, recreation turns to the huge amount of natural resources in the corridor. Hiking trails range from an easy walk



along the Mad River Greenway to the expeditious Long Trail in the Green Mountain National Forest. Biking is popular in the mountains and along Routes 100 and 17 as well. The Mad River provides swimming, boating, and fishing opportunities. Municipalities, resorts, and private enterprises continue to

provide even more recreational opportunities.

The Mad River Byway Extension corridor contains extensive trail systems on public and private lands that climb mountains, travel alongside brooks, and cross through farm fields. They are ideal for walking, hiking, and biking. The Mad River Path Association is a non-profit organization that has taken on the responsibility of developing and maintaining trails in the area. The Mad River Greenway, located in Waitsfield provides an easy path for walkers and bikers over a 6-mile trail system. The Waitsfield and Warren Village Paths are popular walks as well. Camel’s Hump State Forest and the Phen Basin Wilderness host more challenging hiking trails.

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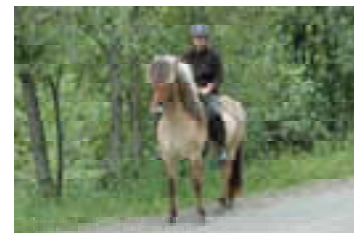
The Hedgehog Brook Trail in Fayston climbs Burnt Rock Mountain and connects with Vermont's Long Trail, which runs along the western boundary of the corridor, including the Cowles Cove Shelter in Fayston. Many paths and undeveloped roads are open to mountain biking, as are downhill ski trails and chair lifts on the major ski hills. Routes 100 and 17 also serve as a popular road biking route, especially through the scenic Granville Gulf Reservation.

The Mad River and its tributaries are the focus of many warm weather recreational opportunities. There are over 43 swimming holes in the Byway corridor, including the publicly owned Picnic Area Cascades south of Warren Village and



Lareau swim hole in Waitsfield. Most of the swim holes are located on private property, leaving the publicly owned ones to experience overuse, which threatens water quality. The river provides whitewater canoeing and kayaking in the spring with popular paddling from the Lareau swim hole to the Meadow Street Bridge. Blueberry Lake in Warren is popular for family paddling outings. The Mad River is also home to moderately healthy fish populations. Limited stocking of brown and rainbow trout, and native populations of brook trout make the river an appealing fly-fishing destination.

Town recreation parks have become more prominent in the corridor in recent years. In Waitsfield, the Lareau swim hole, Munn Field, and ballfields at the Elementary School are important public resources for recreation and river access. The Couples Club, a private not-for-profit organization, maintains playing fields and river access for public use. The Town of Warren owns the Riverside Park, with access to the river, as well as Brooks Recreation Field, home to sports teams, school use, and the site of the annual 4th of July Celebration. Sugarbush Resort opens its 18-hole golf course and tennis club during the skiing off-season. The Bridges Resort in Warren has a fully functioning tennis club as well. Special community events during the warmer months include the Sugarbush Triathlon and the Mad Dash roadrace. Other recreational resources in the Byway corridor include: the Sugarbush Polo



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Club, the Mad Birders guided birding walks, gliding at the Warren/Sugarbush Airport, horseback riding at area stables, picnicking, guided backcountry trips, and hunting and leaf peeping in the fall.

Cultural Resources

Introduction

The Mad River Byway Extension corridor supports rich cultural resources. From arts to agriculture, there are many opportunities to experience the quintessential Vermont culture that exists in the Mad River Valley. The cultural resources and activities in the corridor are generally geared



toward public involvement. Residents and visitors have a variety of cultural activities to choose from: they can take in the Mad River Chorale performances and artwork at the Bundy Center for the Arts; peruse fresh produce at the Mad River Green Farmer's Market; learn about the region's history at the General Wait House; or

attend Warren's Annual Fourth of July Parade and Celebration. Cultural resources often overlap with historic resources and may serve as focal points for scenic resources. Historic Districts in the villages are cultural resources that cater to interested visitors and numerous inns and bed and breakfasts provide a place for them to stay, while serving as a way of life for many corridor residents. The cultural options are seemingly endless.

Agriculture is an important cultural feature of the Byway, signifying the rural traditions and working landscape of the region. Many family farms in the corridor have sold their development rights to the Vermont Land Trust or other conservation organizations, preserving the character of the



farms and integrity of this historic landscape feature. Working farms dot the hillsides with iconic Holsteins and provide the Valley with fresh produce at the Mad River Green Farmer's Market. The Defreest Farm grazes its cows over hundreds of acres, maintaining open meadows that provide the long distance views

characteristic of the corridor. The wooded hills are a source of sap for maple sugaring, a strong tradition still very much alive in the corridor.

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The natural beauty of the Mad River Byway Extension has long attracted and inspired artisans of all disciplines. An active arts community, encompassing theater, music, sculpture, glass blowing and Vermont crafts, attracts visitors and enhances the quality of life in the local Byway communities. The Bundy Center for the Arts, in Waitsfield, exhibits international and local artists' work and is home to the Mad River Chorale. Waitsfield is also the center of the Vermont Festival of the Arts, an annual event that opens artists' studios to the public and encourages residents and visitors of all ages to experience art.



Cultural resources along the Byway include:

Moretown (from 100B intersection)

- Bis-May Farm
- Camels Hump View Farm
- Caravan Bed and Breakfast
- Church of the Crucified One
- Mad River Valley Rural Historic District

Fayston

- | | |
|---|--------------------------------|
| - Fayston Historical Society | - Perly Boyce Farm |
| - Fayston School | - Tenny Farm |
| - Green Mountain Valley School | - Tucker Hill Inn |
| - Hyde Away Inn | - Vasseur Farm |
| - Shepard Brook Talc Mine Site (historic) | - Vermont Icelandic Horse Farm |
| - Knoll/McLaughlin Farm | - White Horse Inn |

Waitsfield

- | | |
|------------------------------|---------------------------------|
| - 1824 House Inn | - Commons Group Theater Company |
| - Artisan's Gallery | - Dana Hill Stable |
| - Blacknewt Sculpture Studio | - Eclipse Theater |
| - Bundy Center for the Arts | - Featherbed Inn |
| - Cabin Fever Quilts | - Federated Church |
| - Carpenter Farm | |

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- General Wait House
- Green Mountain Cultural Center
- Green Mountain Opera Festival
- Harthorn's Farmstand
- Irasville Village
- Irasville Country Store
- Joslin Memorial Library
- Kenyon Farm
- Lareau Farm
- Mad River Glass Gallery & Studio
- Mad River Green Farmer's Market
- Mad River Business Park
- Mad River Valley Craft Fair
- Mad River Valley Rural Historic District
- Mountain Valley Farm
- Mountain View Cemetery
- Mountain View Inn
- Neill Farm
- Our Lady of the Snows Catholic Church
- Pine Brook Covered Bridge
- Richards Farm
- Rose Farm
- Round Barn Farm
- Simplicity Farm
- Spaulding Farm
- Sweet Pea Natural Foods
- The Skinner Barn
- The Store
- The Valley Players Theater
- United Church of Christ
- Vermont Festival of the Arts
- Veteran's Memorial
- Von Trapp Greenhouse
- Waitsfield Common Historic District
- Waitsfield Elementary School
- Waitsfield Pottery
- Waitsfield Village Covered Bridge
- Waitsfield Village Historic District
- Waitsfield Historical Society
- Weathertop Mountain Inn
- Wilder Farm Inn
- Yellow Farmhouse Inn
- Yestermorrow School

Warren

- Beaver Pond Farm
- Blueberry Lake Dam
- Bridges Resort
- Brooks Dam
- Burling Farm
- Deer Meadow Inn
- Defreest Farm
- Dirt Road Company
- East Warren Cemetery
- East Warren School House
- Ford-Rodgers Farm
- Fourth of July Parade
- Golden Lion Riverside Inn
- Kingsbury Iron Bridge
- Mad River Rocket
- Parade Gallery
- Pitcher Inn
- Powderhound Inn
- Rootswork
- Simpson Farm
- Strattonwald Dam
- Sugar Lodge
- Sugarbush Inn
- Sugarbush Triathlon
- Sugartree Inn
- The Common Man Restaurant
- The Creators Shop
- The Warren Store
- Three Shepherds Farm

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- Timber Crib Dam
- Wadhams Stables
- Warren Covered Bridge
- Warren Elementary School
- Warren Public Library
- Warren Pottery
- Warren United Church
- Warren Village Cemetery
- Warren/Sugarbush Airport
- West Hill House

Cultural resources in the Byway are not limited to the listings here. Perhaps one of the most important are not events or buildings or anything you can list, but are the people themselves. The rural character of the Valley's culture, as stated in the Warren Town Plan, is based on "**A rural lifestyle**, marked by relative privacy, peace and solitude; access to the land and nature; a lack of formality; and a strong sense of independence and individualism that is coupled with, though sometimes at odds with, a perception of community spirit and shared responsibility." Whether a vital part of everyday life or a unique opportunity to enrich it, the Mad River Byway Extension is rich in resources that support this lifestyle.



Historic and Archaeological Resources

Introduction

The Mad River Byway Extension encompasses an array and abundance of historic resources, which tell the story of the region's development along the River corridor. The majority of the resources are historic buildings and settlement patterns, still intact today, although some archaeological artifacts dating back to pre-contact Native Americans have been identified as well. These resources connect local residents to their town's history and



captivate tourists. When visitors make the connection between historic sites and present conditions, they develop a greater appreciation of the history of the area and all of Vermont.

Most of the region's historic elements are accessible to all, with buildings casually viewed from the outside or in some cases used for community events. For example, the historic General Wait House in Waitsfield is a visitor information center; the East Warren School House contains space for community classes and workshops; and, the tourist-friendly nature of both towns allows for significant viewing of the historic district buildings. Access into private homes may be restricted, but may be easily viewed from the outside. The Waitsfield Historical Society has developed a walking tour for this purpose and is available at the General Wait House.

According to the Vermont Division for Historic Preservation, prehistoric sites are expected to be located near surface water resources, especially on major floodplains. Other likely locations include high-elevated terraces (over 700') with a commanding view of the underlying valley. Using an extensive model featuring these and other variables, the DHP has mapped areas of potential archaeological significance in the Mad River area including long stretches of land along the Mad River Byway Extension. The few archeological resources found in the area were located on private land and are not accessible to the public. The state also wishes to protect these sites and does not publicize precise locations.

A brief summary of key historic and archaeological resources located in each of the Byway communities is provided below. Much of the

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information was obtained from the VT Division for Historic Preservation-Historic Sites and Structures Survey and the Mad River Watershed Barn Inventory. A complete list can be found in the *Matrix of Intrinsic Resources* at the end of this section.

Moretown, Vermont

A small portion of the Byway is located in Moretown, from the Waitsfield town line to the junction with 100B. This portion is part of the federally listed Mad River Valley Rural Agricultural District and is home to several historic barns, including the Maynard's Bis-May Farm. The District was created to recognize the area's successful farming since its settlement around 1790. Moretown contains several historic mills, the remains of the industry's strong importance along the Mad River. This section of the Byway is also archaeologically significant with the documented identification of a prehistoric chert projectile point, dating back to the Paleo-Indian period (10,000-7500 BC).



Waitsfield, Vermont

The Mad River Valley Rural Agricultural District extends south into Waitsfield. This area, with its vast floodplains, is home to many historic barns that represent the peak of the agricultural tradition in the Byway. These historic farmsteads feature houses and large barns dating back to the 19th century, with vernacular, wooden, gable-roofed architecture. Some Greek and Gothic Revival architecture is present as well. Notable farms in the District include the Simplicity Farm, Carpenter Farm, and Woliner-Neill Farm. The Simplicity Farm and Woliner-Neill Farm have been conserved through the Vermont Land Trust while the Carpenter Farm now doubles as an Inn. Conservation and adaptive conversion of historic farmlands are common in the Byway. Other significant historic farms in the Byway include Kenyon's Barn, the



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Lareau Farm- now an Inn and home to American Flatbread, the Mountain Valley Farm, The Skinner Barn- used today as a theater, and the Joslin Farm- nationally-listed for its Round Barn. In total, 74 barns of historic significance have been identified in Waitsfield.



Other important historic resources include The Waitsfield Village Historic District with its 71 historic homes, stores, and public buildings. Many historic structures now house businesses and offices and are accessible to the public. The Waitsfield Historical Society

has put together a walking tour of the district, which is available at the General Wait House, the first frame house built in Waitsfield and home to the Historical Society. Most of the architecture is a Greek Revival style and notable buildings include the Waitsfield Federated Church, the Joslin Memorial Library, the Masonic lodge, the Bridge Street Market Place and the Great Eddy Covered Bridge. The Waitsfield Common Historic District is another important resource and is made up of the first non-native settlement in the Valley, dating back to the 18th century. It contains vernacular Federal-period houses, a farmstead, a cemetery, and a public

common. Waitsfield also contains the nationally listed Pine Brook Covered Bridge- the oldest operating covered bridge in the state.



Two prehistoric sites have been reported in Waitsfield. A Native American paleo-point has been documented as well as evidence of fire-

cracked rocks used to make arrowheads. This represents the extent of known archeological resources in the Byway. With the Byway's limited development, it is likely that other sites exist but have not been found.

Fayston, Vermont

With its mostly mountainous terrain, Fayston developed around natural resource extraction and subsistence agriculture. There were a number of

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large lumbering operations with sawmills present along the Shepard and Mill Brooks in the 1800s. A talc mine was also located in the Shepard Brook area. None of these buildings remain today. The first settlement in the town was on Bragg Hill land that is now the Vasseur farm. The farm was Fayston's last operating dairy until recent years. Another historic farm is the Knoll/McLaughlin Farm on the east side of Bragg Hill, which has been farmed for over 200 years. This farm is Fayston's only property on the National Register of Historic Places, listed as the McLaughlin/Knoll Farm Historic District, and the first in the town to be conserved with the Vermont Land Trust. It consists of two 19th century bank barns, a 1904 farmhouse, and several other supporting structures. In 1979, the State of Vermont identified a total of 39 buildings and structures with historic significance in Fayston. Notable historic structures include: two original school buildings, Fayston School #4 and Fayston School #9, built in 1860; the original Town Clerk's office, still in use; and the single chair lift at Mad River Glen, the last remaining in the lower 48 states.

Warren, Vermont

Route 100 crosses through the Town of Warren, taking a western curve around the town center. The state has identified over 100 historic properties in Warren. More than 50 of these are located in the nationally listed Warren Village Historic District. The village has been developed slowly over the years and a variety of architectural styles are present in the district. The center of the village consists of the United Church of Warren, Village Cemetery, Town Hall, and Municipal Building (originally the Village School House, now housing the Warren Public Library). Other significant structures in the District are the former Warren House Hotel- now the town's general store, and the Pitcher Inn- originally built in 1850 and restored after a fire in 1993. Warren was a strong mill town from 1820-1940. Today only foundations remain of the mills, many washed out by the Flood of 1927 or consumed by fires.



Warren contains two historic bridges. The Warren Village Covered Bridge is also a nationally listed structure and is a proud symbol of the town. Route 100 crosses the Mad River over the Kingsbury Iron Bridge,

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the sole remaining iron truss bridge in the Valley. The Warren Village timber crib dam is also of historic significance and is currently in need of renovations after being rebuilt twice since the 1927 flood. It is a lasting symbol of Warren's mill industry.

Like the rest of the Byway, Warren is home to many historic barn the Defreest Farm- the Valley's largest dairy farms, and schoolhouses as well. Of note is the East Warren School House, currently the home of the nonprofit Rootswork, and a local market. Other schoolhouses have been converted into private homes. There are numerous historic farms and include the Kathan Farm, Senor Farm, Eurich Farm, and the Elliott Farm. Like the other valley communities, many farms have been converted to inns or conserved through groups like the Vermont Land Trust.

Scenic Resources

Introduction

The Vermont Byways program provides a basis for evaluating scenic resources along a Byway corridor. This methodology is described in the “Scenic Evaluation Process” section of the Vermont Byways Program Manual, *Designating Vermont Byways: A Fieldguide*. A defined section of the proposed Byway is given a rating ranging from a -3 (very negative), having a landscape quality that is uncharacteristic of Vermont, and which greatly detracts from the visual quality or experience of the Byway corridor, to a rating of +3 (very positive), which might include a scenic resource unique to Vermont or New England, such as long distance or sweeping view of the mountains uninterrupted or not undermined by foreground elements. A Byway corridor must have an overall rating of at least +1 to qualify for the program and the Mad River Byway Extension, with no negatively rated sections and consistent high ratings definitely meets and exceeds this requirement.

The highest rated (greater than +2.5) scenic resources in the corridor include: the area just south of the 100/100B intersection with views of the Northfield Range, working dairy farms, meadows and the river creating very pleasing views; the historic 1824 House Inn and surrounding views of the Green Mountains to the east; the entire Granville Gulf area at the southern end of the Byway; and Route 17 with historic character in the beginning, views of Mad River Glen in the middle all culminating at the top with the most spectacular views on the Byway. These are all outstanding resources and exemplify the rural, historical, and natural character of the area. It is important to note that there are no negatively rated sections on this Byway, with the lowest-rated section being a +1 at the Irasville commercial area and at the developed area just south of the Warren Village entry. Views here are still positive though, and do not detract from the overall scenic beauty of the area. The Byway’s consistent ratings of +1.5 to +2.5 are due to the following characteristics:

- 1) The presence of the Mad River along most of the route, which provides a unique opportunity to experience a highly scenic Vermont river with many engaging visual, natural and recreational resources;
- 2) The steep and narrow nature of the Valley, which creates a dramatic backdrop of the surrounding mountains in most views;
- 3) The intact settlement pattern of old homes, barns, farmsteads and villages which capture and present a pleasing and historic land use pattern which is fast disappearing elsewhere in the state and nation; and,

- 4) The overall and quintessential Vermont landscape quality that is present from one end of the proposed corridor to the other presents an intact landscape quality and integrity that is worthy of recognition, management and preservation.

Route 100 Section by Section Analysis

Mile 1.0-2.0

The beginning of the proposed corridor, south of the intersection with 100B, is one of the more scenic sections of the Byway, capturing the classic Vermont countryside. Views progress in this rural residential area from the river and farms in the foreground to meadows and the Northfield Range, including Scragg Mountain, in the background. As the Route crosses into Waitsfield, open meadows and fields are framed by spectacular background views to the southwest of the Green Mountain Range, and full, sweeping views of the Northfield Range.

The intact nature of this segment, the landscape variety, and the long distant views combine to suggest a rating of 2.5-3 for this segment.

Mile 2.0-2.5

This section, south of the North Fayston Road intersection features the Mad River in the foreground, curving alongside the road. Agricultural fields roll out in the mid-ground. Views of a rock outcrop to the west convey the wild geologic character of the Valley. Towards the end of this section, the historic Wilder Farm Inn shows an example of traditional farmstead settlement.

Visual access to the riverscape, long distance views, and the relatively intact pastoral landscape with a sense of the working nature and history of this area warrants a rating of 2.

Mile 2.5-3.0

This section, from the intersection of Meadow Road, is bounded with roadside vegetation preventing any significant views. It is more settled, with mobile homes detracting from the viewing experience.

Although still a pleasing section of the road, without any significant features besides modern development this section rates a 1.5.

Mile 3.0-3.5

Through this section, with its rising topography, one has expansive views to the east and of the mountains in the background, while pastoral meadows are prominent in the mid-ground. Representing the rural

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agricultural character of this region is the Hartshorn's Farmstand and historic 1824 House Inn, an original New England style post and beam structure.

This section exemplifies the mesh of Vermont's historic working landscape with the contemporary importance of the farming industry in the area. Long-distance views of the mountains add a dramatic touch to this feeling and with the intactness of this section create a high quality scenic resource. Rating of 2.5-3.

Mile 3.5-5.0

The approach to Waitsfield village presents high quality Vermont views. The Green Mountains rise up to the west and classic old barns and homes dot the landscape. The river is visible from the road and the steeple of Waitsfield can be seen in the distance. The picnic area and commercial uses around Kenyon's farm allow opportunities to access the landscape without detracting from the view. Open meadows and pine plantations are reminders of the working nature of the landscape. Just before entering the village, hillside homes and the school create a more modern yet visually pleasing settlement pattern.

This section reflects the relationship of human history and the evolving landscape. The pine plantation and meadows are a legacy of historic land uses, while the homes and school represent the modern uses. This section also features a classic Vermont long-distance view of the village steeple, invoking the sense of place and importance of the village ahead. This is a long and intact stretch of the representative Vermont landscape and warrants a rating of 2.

Mile 5.0-5.5

This section passes through the historic village of Waitsfield, south of the Old County Road intersection. The village, with its intact and original character, is a high quality historic and scenic resource to the state. The history of Waitsfield is readily apparent when viewing the historic buildings and their corresponding architecture pattern lining the road. Street trees add to the beauty of the intact village. The only intrusions are the utility poles, which detract slightly from this quintessential Vermont village.

Waitsfield village provides the traveler and resident with a strong sense of a Vermont village developed in a linear fashion alongside the river corridor. The presence of engaging architecture, an historic village settlement pattern, and fully intact and functional historic buildings create an outstanding, if not unique, visual experience along this segment. It can be particularly

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appreciated if accessed by foot using the Waitsfield Village Historic District Walking Tour available from the Waitsfield Historical Society in the Wait House. While the utility poles are a slight negative, this section still rates a 2.5.

Mile 5.5-6.0

As you enter Irasville, south of Radcliffe Drive, the scenic quality is still pleasing but decreased slightly, due to the commercialized sections of the village.

This typical yet pleasing commercial section rates a 1.5-2.

Mile 6.0-6.5

The continuation of Irasville becomes more developed with a gas station and shopping center. The hills in the background and river on the right maintain some scenic quality in this section.

This stretch, while not particularly unique does feature views of the river and rates a 1.

Mile 6.5-7.0

This section, just past the intersection with Route 17, passes by the Fiddler's Green and features river views as the mountain ridges rise up in the background. The section is less developed and passes through mixed forests as the Route follows the Mill Brook then returns to the Mad River. Towards the end of this section, nice berming is visible at the public Lareau swimming hole.

With limited development, diverse views of natural resources, and scenic public access points, this high quality segment warrants a 2.5 rating.

Mile 7.0-8.0

Through this section of road, which crosses and moves away from the Mad River, successional fields are visible to the west and mixed development to the east. Views of open meadows and the historic Hap's Garage, a local landmark circa 1876, are typical components of the Vermont landscape. While roadside vegetation closes in alongside the southern part of this section, open views of ridgelines and meadows remain visible. Some commercial development is present to the east.

This section features a variety of landscape qualities and nice background views, similar to the previous section. The increased development here detracts from the otherwise intact nature of this section and decreases the rating to a 2.

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Mile 8.0-8.5

This section starts with the intersection with Bundy Road and passes through vegetated areas before passing the Yestermorrow School. The views open up somewhat to undeveloped meadows and rolling bumps of hills.

This is a scenic but not a particularly unique section of road that warrants a 1.5 rating.

Mile 8.5-9.0

A roadside Citgo station is present in this segment as are beautiful views of the hills, both near and far, and open meadows at the river's edge. The old iron Kingsbury Bridge brings Route 100 across the river as the surrounding topography begins to rise.

With nice open views of the mountains, river, and meadows, as well as the historic bridge, this section warrants a rating of 2 for it's pleasing view of the Vermont landscape.

Mile 9.0-9.5

This section passes by the Sugarbush Access road, and through an area with some roadside development including lodging establishments and a used car lot with service station. This is also a site for a future visitor center.

Commercial development and an absence of high quality views make a 1.5 rating appropriate for this section.

Mile 9.5-10.5

Views open up to more natural scenery including nice views of the river bend and evergreen stands in the foreground, hills and ledges in the mid-ground, and long range views of the mountains in the background. The section also passes The Seasons with its manicured open lawn.

This section has many qualities typical to the Vermont landscape, but the near view of the ledge and the landscaping around The Seasons adds variety to the view. These pleasing views warrant a rating of 2.

Mile 10.5-11.0

Entering Warren village, the road is framed by vegetation. There are no long views although hillside views are visible in the mid-ground.

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This is a typical and pleasant view but lacking any outstanding features or focal points results in a rating of 1.5.

Mile 11.0-11.5

This section is similar to the one before, with vegetation close to the road and some development. A nice view of the hillsides is visible towards the end of the section before it crosses the river.

The majority of this section, lacking any interesting views, warrants a rating of 1, while the view of the hillsides warrants a rating of 2.

Mile 11.5-12.0

An old factory in this section conveys the historic industrial activities of the Valley. Vegetation close to the road limits views but then opens up to a major rock outcrop and nice views of nearby hills. Beautiful views of the Mad River as it runs by the road add to the scenic quality of this section.

This stretch, while not unique, still is a beautiful and engaging landscape that combines views of high quality cultural and natural features for a rating of 2.

Mile 12.0-12.5

Vegetation close to the road screens the surrounding development in this section. A pull-off provides drivers with an opportunity to stop and appreciate the surrounding views of the mountain ridges.

This section warrants a rating of 2.

Mile 12.5-13.0

Close vegetation continues into this section, framing the road. Development is not very visible although some historic homes can be seen from the road in this view of a classic Vermont landscape.

This section has many of the same qualities as previous section. The distinctive architecture also conveys a sense of the Valley's history and warrants a rating of 2.

Mile 13.0-13.5

This section is a beautiful example of Vermont's rugged natural features. Rock outcrops are visible above the road while ridgetop views extend in the background. The section crosses the river and confluence with Stetson Brook. The lack of development creates a high quality scenic resource.

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This section with the rock outcrops, long distance views, and intact, undeveloped landscape warrants a rating of 2.

Mile 13.5-15.0

More rock outcrops and a lack of development are visible in this section. Evergreens frame the route in some sections as the river winds in and out of sight and under the road. Mature forests in the foreground close-in along the end of this section. A picnic area in the middle of the section provides a chance for more close up views of the forest.

This intact and wild portion of the route is a high quality scenic resource. While closed-in vegetation limits long distance views, the river is just as beautiful especially when framed by the mature forests and rock ledges. Rating of 2.5.

Mile 15.0-21

The river runs very close to the road as you enter the Granville Gulf Preserve. Spectacular views surround the road in this area conserved for its beauty. High ridges in the background frame evergreens and rock outcrops. There are many pull-offs as the road winds around wetlands and spruce stands. As the road starts to go downhill, it passes through a very narrow gorge with ledges on both sides. Access to Moss Glen Falls is a beautiful sight in itself. As the Preserve ends, the landscape flattens out as the river meanders through a broader valley. The view opens up to distant peaks and broad floodplains.

Completely intact and untouched by development, this section is spectacular and unique. The variety of natural features, from rock outcrops to wetlands to waterfalls, adds to the beauty of the Mad River at its source. With numerous access points this section warrants the highest rating, a 3.

The combined rating for all segments in the Route 100 section of the proposed Mad River Byway Extension is **2.3**. This rating is very high and represents the strong presence of the scenic qualities of the corridor.

Route 17 Section by Section Analysis

Mile 0.0-0.5

This section, just West of the Route 100/Route 17 intersection, is settled with old homes and trailers. There is also a Valero gas station. The Mill Brook runs right beside the road and foreground views include a waterfall, the wooded forest canopy and an old millyard, which still operates much as it did in the past. The mountains above are visible in the distance.

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This section, the most commercialized of Route 17 warrants a rating of 1 to.

Mile 0.5-1.0

As you move away from the major intersection, natural features become more prominent in the views as the Mill Brook continues to run alongside the road. In the foreground, old barns and homes provide an historic presence. The wooded canopy remains a prominent feature and long distance views open up to Sugarloaf and a ridge on the east side of the valley.

This naturally beautiful section with interesting near and far views, historic features, and landscape variety warrants a rating of 2.

Mile 1.0-1.5

Continuing up the mountain, the route passes by an old school house, open meadows, and beautiful, stately pines. The forest canopy rises up on both sides, occasional clearings provide glimpses of the mountains. Towards the end of this section, the forest opens up to views of the Mad River Glen ski area.

Natural beauty dominates through this section and the views of Mad River Glen are a symbol of Vermont's exceptional recreational resources. This combination of resources warrants a rating of 2.5.

Mile 1.5-2.0

The views continue in this section, with more interesting foreground elements incorporated into the view. An old farmhouse and apple trees provide a glimpse into the area's agricultural past, while large pine stands continue to tower over the natural vegetation. The road crosses the brook, with views of the water rushing by as the brook continues to run alongside the road.

The integration of historic and natural elements is pleasing and interesting. Direct views of the brook add to the quality of this section, which is rated 2.

Mile 2.0-4.0

In the beginning of this section, the close canopy vegetation opens to meadows and pastures, a further reminder of the importance of agriculture in the Valley, even at the higher elevations. Continuing on, younger trees rise up along the roadside as well as another pine plantation, evidence of the region's logging industry. Natural features dominate the foreground with a long distance view of the Mad River Glen ski area and surrounding ridgelines. This section features recreational access points with the Mad River Barn and access to the Catamount trail. The forest

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continues alongside the road with the canopy reaching over both sides. An open field provides a long distance view to the ridge.

With limited development, diverse views of a variety of natural resources both near and distant, reminders of the region's historic industries, and recreational access points, this high quality segment warrants a rating of 3.

Mile 4.0-5.5

The deep woods dominate the start of this section as the road cuts through the forest. Breaks in the forest provide continual glimpses of the ridge of Mad River Glen. The Mill Brook returns to run alongside the road in this section. As the road reaches the summit, a clear view stretches out to the ski area and mountaintop. In the foreground, an exposed rock ledge rises up alongside the road and at one point a river gorge is visible. The route passes the Mad River parking lot with its wide open shoulder. Continuing past the Mad River Glen base, views to the ridge are prominent. The road also passes the top of the Mad River Glen chairlift.

This segment of Route 17, as it climbs the mountainside, has many scenic views. Mad River Glen Ski Area, one of the corridor's most important recreational resources is accessible here. Rock ledges and a river gorge add variety and interest to the foreground elements. While the parking area for Mad River Glen stretches out directly alongside the road, this segment is highly scenic and warrants a rating of 2.5.

Mile 5.5-6.0+

As the route reaches the Byway boundary, it rises steeply up switchbacks in this scenic section. Lower vegetation allows for a straightaway view to the gap and long-distance views across the valley to the Northfield Range. The summit of the Route 17 Gap has spectacular near and far views. A major ledge drops down to a mountaintop pond and wetland area. Large pull-offs provide access to viewing overlooks and parking for the Long Trail, which crosses the road. Low vegetation on both sides of the road exposes vertical rock faces. As the route begins to descend, the pond is more visible, just over the guardrail.

Spectacular views both east and west are visible over low-lying vegetation as the route approaches and reaches the summit of the Route 17 Gap. At the summit, a beautiful mountaintop pond is a surprising feature and pulloffs provide a view straight down into the water. Vermont's ultimate trail, the Long Trail, passes here and the pulloffs provide parking and access for both day and thru travelers. With amazing views, both near and far, this section is the most scenic of the route, with a rating of 3.

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Route 17, as part of the Mad River Byway Extension is an important scenic resource. The Mill Brook follows and criss-crosses the road for much of its route. Passing in and out of the canopy and in some places through deep, native woods, Route 17 has an average rating of **2.5**.

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Matrix of Significant Corridor Resources

The matrix shown below provides an inventory and listing of all the resources located throughout the entire byway and delineates them according to one or more of the six intrinsic categories outlined in the State Byways Program.

	Archaeological	Cultural	Historic	Natural	Recreational	Scenic	Notes
Moretown							
Arti Shed Dogs					*		
Bis-May Farm		*	*	*			Conserved land
Camels Hump View Farm		*					1850s farm house - 8 rooms
Caravan Bed and Breakfast		*					Specializing in local food and activities - 5 rooms
Green Mountain Range						*	View west
Mad River Valley Rural Historic District			*				
Moretown Gorge				*		*	
Mountain View Cemetery		*					
Northfield Range						*	View south
Previous Ward Lumber Mill			*				
Projectile Point Found	*						
Proposed Bike Path					*		
VAST Trail					*		
Village Cemetery		*					
Ward Chair Stock Mill			*				
Worcester Mountain Range						*	View north
Fayston							
Bicknell's Thrush Habitat				*			
Big Basin Forest Trust				*			Conserved land, restricted development
Black Bear habitat				*			Much of forest land, especially Slide Brook Basin
Boyer Property				*			Land conserved by VLT
Bragg Hill						*	
Brightonbeck Property				*			Conserved land adjacent to Knoll Farm
Burnt Rock Mountain				*	*		Hiking
Camel's Hump State Forest				*	*	*	Access in Fayston, State Forest extends into Waitsfield
Camel's Hump State Park				*	*	*	Part of SP land in Fayston; access in Huntington
Catawampus trail					*		to ski
Center Fayston Cemetery		*	*				
Center Fayston Road						*	
Chase Brook conserved land				*	*		Deer and bear habitat, trails
Deer Brook				*			
Deer Wintering Yard				*			Steeply sloped areas adjacent to lower Shepard and Mill Brooks
Farnsworth Property				*			Conserved land
Fayston Historical Society		*	*				
Fayston School		*			*		
Flower Property				*			Conserved land
French Brook				*			
Hedgehog Brook Trail					*		
Hemlock Hill				*		*	
Hemlock Hill Trail					*		
Hobart Barn			*				
Huntington Gap Wildlife Management Area				*			
Knoll/McLaughlin Farm		*	*			*	Listed on National Register, conserved land, scenic views
L. Conrad-Graves Barn			*				
Levy Barn			*				

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	Archaeological	Cultural	Historic	Natural	Recreational	Scenic	Notes
Livingston Farm			*				
Lockwood Brook				*			Used by Sugarbush for snowmaking
Mad River Business Park		*					Off N. Fayston Rd
Mad River Glen ski area					*		
Mill Brook Trail					*		
Perly Boyce Farm		*		*			Conserved land contains prime scheduled wildlife habitat
Phon Basin Wilderness				*	*		beaver ponds, other wildlife habitat, trails
Rte 17 to Appalachian Gap						*	
School Number 4			*				Currently private home, c1860
School Number 9			*				c1860
Shepard Brook		*	*	*			Site of old tail mine
Slide Brook				*			
South Fayston Cemetery		*	*				
Stark Mountain				*	*	*	
Sugarbush North Ski area					*		access to Mt. Ellen
Tenny Farm		*					
The Long Trail					*		
Town Clerk's Office			*				Original building
Vaseur Farm		*					
VAST Trails					*		
Vermont Icelandic Horse Farm		*			*		
White Horse Inn		*					
Williams Dam		*					
Waitsfield							
1824 House Inn		*	*				
Alice Post & Beam		*					Builds post and beam homes etc. in tradition of VT craftsmen
Andrew Bigelow House			*				c1840
Artisan's gallery		*					VT crafts and artists
Austin Stoddard Farm			*				
Band Saw Mill Shed		*	*				Originally part of saw mill, early 19th century
Bald Mountain				*		*	
Barnard-Messer Farm			*				
Blackwell Sculpture Studio		*					
Blacksmith Shop			*				c1838, historic building
Bundy Center for the Arts		*					Mad River Chronicle, art
Bridge Street Camoo Access					*		
Cabin Fever Quilts		*					Local quilts
Camoo's Hump State Forest				*	*	*	
Carpenter Farm		*	*				
Cemetery behind Wait House			*				Wait family buried there
Cheney Prentice House			*				c1840
Church of the Crucified One		*					
Clearwater Sports					*		Rentals, Guiding
Congregational Parsonage		*	*				c1845, part of building was originally a school house
Couple's club rec field					*		In town, swimming hole behind field
Covered bridge swimming hole					*		Above bridge, boat access

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	Archaeological	Cultural	Historic	Natural	Recreational	Sonic	Notes
Dan Richardson House			*				c1880
Dana Hill				*		*	
Dana Hill Stable		*			*		
Easy Street Cafe and Market		*					
Eclipse Theater		*					Live music
Edwin Dumas House			*				c1825
Evergreen Place Senior Center		*					
Exxon Service Station			*				Current gas station housed in historic carriage barn c. 1876
Featherbed Inn		*	*				c1806
Federated Church		*	*				c1874 Best example of Romanesque Revival style church in state
Hemer Dairy Barn			*				
Hemer Property				*			Conserved land
French Brothers House			*				c1850
Gaylord House			*				c1860
General Wain House		*	*				Historical Society meeting place, educational exhibits, Chamber of Commerce, visitor center
George Ridder House			*				c1825
Goodman Barn			*				
Great Ledge/Village Covered Bridge			*		*		Nationally listed historic bridge, boat access
Green Mountain Cultural Center		*					At Round Barn Farm, hosts cultural events including Green Mountain Opera
Green Mountain Valley School		*					
Hadley Gaylord House/Barn		*	*				Farm complex, includes c1900 2 1/2 story barn
Hap's Service Station			*				Original service station in valley, still in operation
Hartshorn's Farmstand		*					Organic
Haskin Barn			*				
Hartings S. Campbell House			*				c1845
Haring Spaulding House			*				John Burdick homestead, dwelling c1860, barns c1885-1900
High Bridge Brook				*			
Hiram Jones House			*				c1845
Hobbsville Property				*			Conserved land
Hyde Away Inn		*					
Iraville Cemetery			*				
Iraville Country Store		*					
J. Holden House Barn			*				
J.W. Richardson Estate			*				c1845, aka Village Grocery
Jacob Boyce House			*				c1845
Jay's Restaurant		*					
John Walton House			*				c. 1838
Jones Barn			*				
Joslin Memorial Library		*	*				c1913
Kenyon's Enterprise/Farm		*	*		*		Country farm store, VAST snowmobile access
Kew Hill				*		*	

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	Archaeological	Cultural	Historic	Natural	Recreational	Scenic	Notes
Laneau Farm		*	*				Inn and American Flatbread on premises
Laneau's swimming hole and Rest Area					*		Town owned, boat access
Laundry swimming hole					*		Behind laundromat at Paddler's Green
Luzeta Shaw House			*				Private home c1845
Mad Dash		*			*		Annual footrace
Mad River Glass Gallery & Studio		*					
Mad River Green Farmer's Market		*					May-October
Mad River Greenway					*		ice/ski trail, hiking path, snowshoeing
Mad River Inn		*	*				c1860s
Mad River Valley Crab Fair		*					hosted by Valley Players
Mad River Valley Health Center		*					
Mad River Valley Rural Historic District			*				Nationally listed because of historic farmland and agricultural practices
Maple Ave Farm				*			Conserved land
Masonic Lodge		*	*				c1831, oral display windows cantilevered on granite sills
Meadow Road Bridge swimming hole					*		Downstream of S/Gs. Philips, boat access
Mehuron Barn			*				
Mill Brook				*			
Millbrook Inn & Restaurant		*	*				c1850s
Mount Wainfield				*		*	
Mountain Valley farm		*	*		*		Horse drawn rides, c1882 barn, conserved land
Mountain View Cemetery		*	*				
Mountain View Inn		*	*				
Mt. Alice				*	*	*	Scrag Mountain Municipal Forest
Munn Field					*		
Neill Farm		*					
Old Methodist Episcopal Church			*				c1853
Our Lady of the Snows Catholic Church		*					
Palmer-Wilson House/Farm			*				c1900s, several barns, silo, home
Pine Brook				*			
Pine Brook Covered Bridge			*				Nationally listed
Pingry House			*				c1835-1880
Projectile Point artifact	*						Documented Paleo-Indian artifact
Punch bowl swimming hole					*		Private, access/visitation allowed
Purple Moon Pub		*					
Rakom Road						*	Scenic views along road
Richards Farm		*					
Richardson James Olmson House			*				c1843
Roderick Richardson House			*				c1817, oldest house still in original site

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Roderick Richardson, Jr. House			*				c1834
Rose Farm		*					
Round Barn Farm (Formerly Jordin Round Barn)		*	*		*	*	c1910 Currently an Inn and ski center. Historical Society annual dinner, numerous trails, and great views of the Green Mountains
Roadbury Mountain Nursery School			*				c1856 dwelling
Rte. 17						*	
Scrag Mountain and Municipal Forest				*	*	*	beaver ponds, 300 acres
Second John Walton House			*				c1840
Shepard Brook				*			
Simplicity Farm		*	*	*			Working dairy on National Register of Historic Places, important riparian corridor
Skatnum					*		Public skating rink
Small Dog Electronics		*					
Smith Barn			*				
Spaulding Farm		*					
Steel Truss Bridge			*				Town Highway 29
Stoddard-Austin House			*				Farm complex with factory/shed c1845-1860
Stoddard-Green-McAlister House			*				c1890, vernacular Greek Revival style
Stoddard-Kneeland-Laneau Farm			*				Historic farm complex c1840-1900
Sweet Pea Natural Foods		*					
The Collection		*					Antiques
The Farm on Mad River		*					Inn
The Garroon		*					Lodge
The Old Church			*				
The Old High School			*				Historic building
The Old Tin Shop			*				c1840, used recently as hardware store and electrical shop
The Pines Rest area					*	*	Foot access, state maintained, snow mobile access, views
The Skinner Barn		*	*				c1801, rented out for events, home of Commons Group Theater Company
The Store		*	*				Country store in c1860 meeting house
The Valley Players Theater, org. Union Meeting House, Oddfellow's Hall		*	*				c1830 Union Meeting House, Society Church, currently Theater, music, events, craft
The Very Small Donut Company		*					
Tucker Hill Inn		*					First area inn geared at downhill skiers
Tucker Ward Barn			*				
United Church of Christ		*	*				c1873, Third Congregational Church in Waitsfield
Valley Mead Farm			*				

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	Archaeological	Cultural	Historic	Natural	Recreational	Scenic	Notes
Vermont Festival of the Arts		*					Annual, centered in Wainfield, programs throughout valley
Veteran's Memorial		*	*				
Village Cemetery		*	*				
Village Path					*		Walking paths through town
Von Trapp Greenhouse		*			*	*	Display garden, great views
Wan Farm Motor Inn		*					
Wainfield Common Historic District			*				Nationally listed, off Rt 100
Wainfield Elementary School		*			*		
Wainfield Historical Society		*	*		*		walking tour
Wainfield House		*	*				c1880, site of town meetings for many years
Wainfield Inn		*	*				c1825
Wainfield pottery		*					
Wainfield Village Historic District		*	*				Nationally listed
Walton Farm			*				
Walton Moriarty House			*				c1840
Wentworth Mountain Inn		*					
Wilder Farm Inn		*	*				c1850s
Yellow Farmhouse Inn		*					
Warren							
Arnhem Farm			*				
Austin Brook					*		Forest road unplowed in winter. Starts in Warren, extends into Grotonville.
Beaver Pond Farm		*	*				c1840s, current Inn
Bicknell's Thrush Habitat				*			
Black Bear habitat				*			Area south of Lincoln Gap Road in GMNF, Slide Brook basin between Lincoln Peak and Mount Ellen, and Northfield Mountain range circa late 1800s
Blair Farm			*				
Blueberry Lake (Warren Lake)				*	*	*	
Blueberry Lake as Ski Center					*		
Bobbin Mill Cascades					*		Located behind the Dirt Road Co.
Swimming Hole					*		
Bradley Brook				*			
Bradley House		*					Vermont crafts
Brauer Barn			*				
Bridges Resort		*			*		Family Resort and Tennis club
Brooks Field					*		Town owned
Bulling Farm		*					
Burnt Mountain				*		*	
Buzzell-Whitney Farm			*				
Carter Barn			*				
Cascades picnic area					*	*	Swimming hole state owned, accessed off Rt 100, 3 mi south of Warren Village
Catamount trail					*		
Chez Henri Restaurant		*					
Clay Brook				*			
Curtis Peak				*		*	
Deer Meadow Inn		*			*		Fishing

III. INTRINSIC RESOURCE INVENTORY & ASSESSMENT

Draft for Review 9/11/07

	Archaeological	Cultural	Historic	Natural	Recreational	Scenic	Notes
Deer Wintering Yard				*			Concentrated along the steep valley wall separating the river valley from East Warren, on West Hill, in a narrow band along both sides of Lincoln Brook
Detroitt Farm		*	*			*	Last working dairy in Warren, herds keep meadows open providing scenic views
Dirt Road Company		*					Company produces a variety of wooden furniture, toys, canoe parts, and energy-saving building components
Double Top Mountain				*	*	*	Conservation easement with public access also protection of a deer yard
East Warren Road						*	Scenic views, road extends into Waterfield
East Warren School House		*	*				Contains market that sells Vermont made items, space for community classes and workshops, Warren Grange, nonprofit Rootstock
Elliot Farm			*				
Eurich Pond Land				*			Conserved land
Felsum Brook				*			
Ford Rodgers Farm		*					
Fourth of July parade		*					Held annually since 1948
Freeman Brook				*			
Fuller Farm				*			Conserved land
Fuller Hill Rd						*	Scenic views along road
Golden Lion Riverside Inn		*					
Green Mountain National Forest				*	*	*	several areas, 6,298 acres
Kathan Farm			*				circa late 1800s
Kingsbury Bridge Greenway					*	*	Trail, scenic views
Kingsbury Iron Bridge		*	*				sole remaining iron truss bridge in valley
Knicker's Café		*					
Lincoln Brook				*			
Lincoln Gap Road						*	Scenic views along road
Lincoln Peak				*	*	*	Sugarbush
MacLay Land Conservation Restriction				*	*		Contains deer wintering yard and public access easement
Mad River Rocket		*					Factory store and promoter of backcountry sledding
Martin Ketchel Barn			*				
Mills Brook				*			
Mt. Ellen				*	*	*	Sugarbush
Odyssey Stoneware Pottery and Fine Crafts		*					
Ole's Cross Country Ski					*		
Parade Gallery		*					Vermont artists
Paradise Deli and Market		*					
Pickler Inn		*	*				Rebuilt 1800s structure
Powderhound Inn		*	*				Lodging

III. INTRINSIC RESOURCE INVENTORY & ASSESSMENT

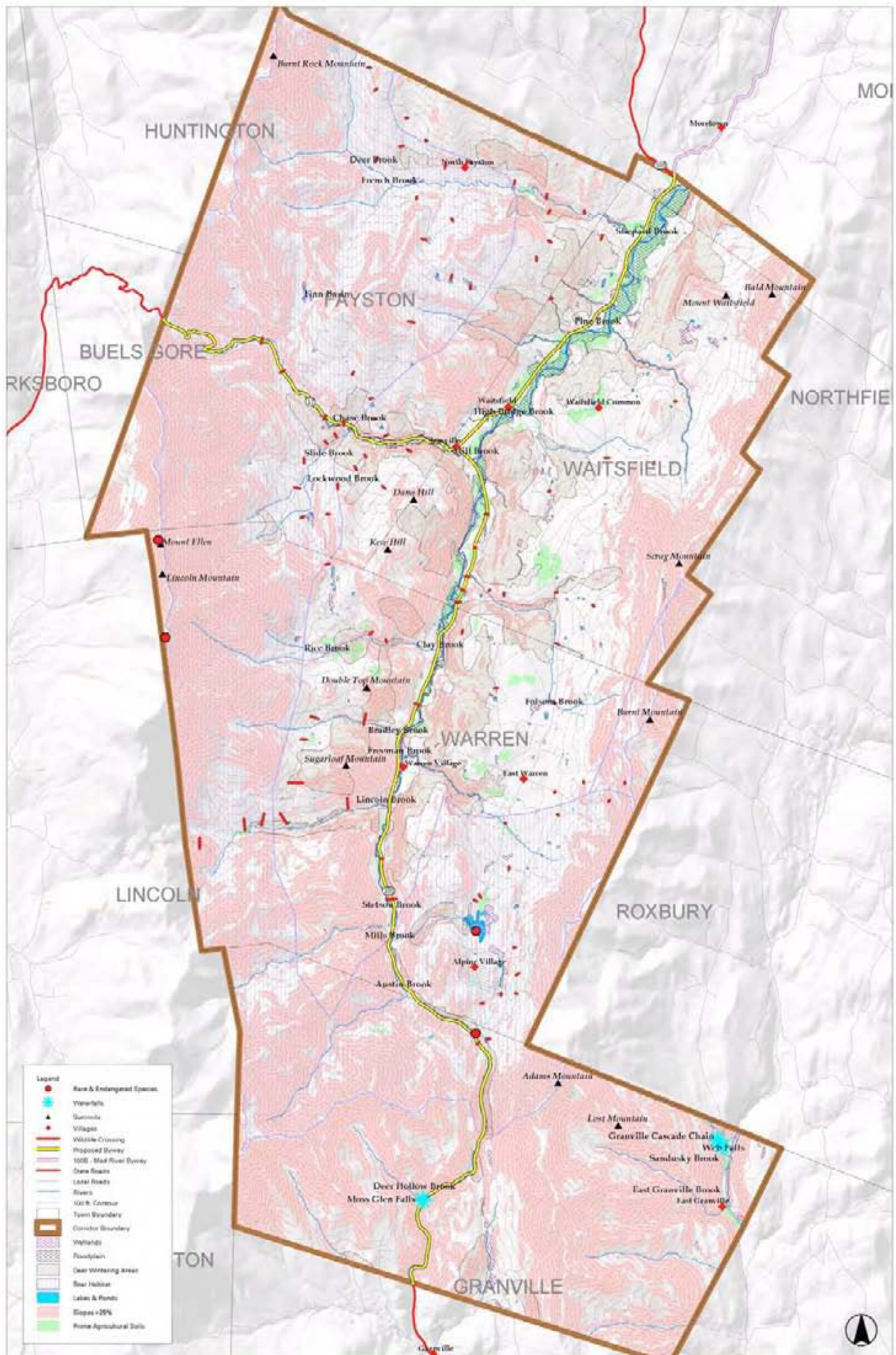
Draft for Review 9/11/07

	Archaeological	Cultural	Historic	Natural	Recreational	Scenic	Notes
Fratt Land				*		*	46.9 acres permanently conserved with VLT, provides remote wildlife habitat
Ralph Phelon Farm			*				
Rare Orchid Species				*			Located at Blueberry Lake
Rare Plant Communities				*			Located on Green Mountain National Forest land
Rice Brook				*			
Riverside Park					*		
Rothbury Mtn Road						*	Scenic views along road
Scout Farm			*				
Scout Road						*	Scenic views along road
Simpson Farm		*					
South Hollow School No. 6			*				Private residence on Lincoln Gap Road, formerly a school
Stetson Hollow and Stetson Brook cascades					*		Forest road unpaved in winter. Starts in Warren, extends into Granville.
Strattonwald Dam		*	*				
Strutt Farm			*				circa late 1800s
Sugar Lodge		*					
Sugarbush Colder Rides and Airport		*			*		
Sugarbush Golf Course					*		
Sugarbush Inn		*					
Sugarbush Ski resort					*		Downtown and cross country ski areas
Sugarbush Trailblazer		*			*		Annual event
Sugarloaf Mountain				*		*	
Sugartree Inn		*					
The Blue Tooth		*					Restaurant
The Common Man		*	*				1850s barn
The Creators Shop		*					Hand-crafted Vermont gifts
The Long Trail					*	*	
The Natural Bridge				*		*	Geologic formation, one of two known arches in Vermont
The Warren Store		*	*				Historic and still operating country store
Thore Sherbrooke Farm		*					Traditional cheese makers
Turners Falls				*			
Warblers Stables		*					
Walter Zinn Farm			*				c1860s
Warren Covered Bridge			*				c1879
Warren Elementary School		*			*		
Warren Falls				*	*	*	
Warren Gorge				*	*	*	Privately owned swimming hole, in Warren village
Warren Lake Dam (Blueberry Lake)		*					
Warren Pinnacle				*	*	*	Mountain, 600's xc ski trails
Warren Pottery		*					Featuring work of local
Warren Recreation Path					*		Trail through town
Warren Riverside Park				*	*	*	
Warren Timber Dam		*					Currently private hydroelectric
Warren United Church		*	*				c1938

III. INTRINSIC RESOURCE INVENTORY & ASSESSMENT

Draft for Review 9/11/07

	Archaeological	Cultural	Historic	Natural	Recreational	Scenic	Notes
Warren Village Historic District			*				Nationally listed
Warren Village Park					*		
Warren Village Swimming Hole					*		Accessed at covered bridge
West Hill House		*	*				Inn, c 1850
Whitworth Farm			*				
Yestermorrow School		*					
Granville							
Adams Mountain				*		*	
Austin Hollow Brook Estement				*	*		
Deer Hollow Brook				*			
East Granville Brook				*			
Granville Cascade Chain				*			
Granville Gull Reservation				*	*	*	An old growth stand of red spruce and hemlock grows on the steep rocky slopes at the north end of Granville Gull
Granville Gull Rest Area				*	*	*	
Green Mountain National Forest				*	*	*	
Lost Mountain				*		*	
Moss Glen Falls				*	*	*	Parking area and trail to this very accessible waterfall
Sandusky Brook				*			
Web Falls				*			



Mad River Byway EXTENSION

DRAFT Nomination Package and Corridor Management Plan

NATURAL QUALITIES MAP

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 Last updated: 10/1/2016

Prepared by: Litchfield
 Date: 10/1/2016
 10/1/2016
 10/1/2016

Mad River Byway *EXTENSION*

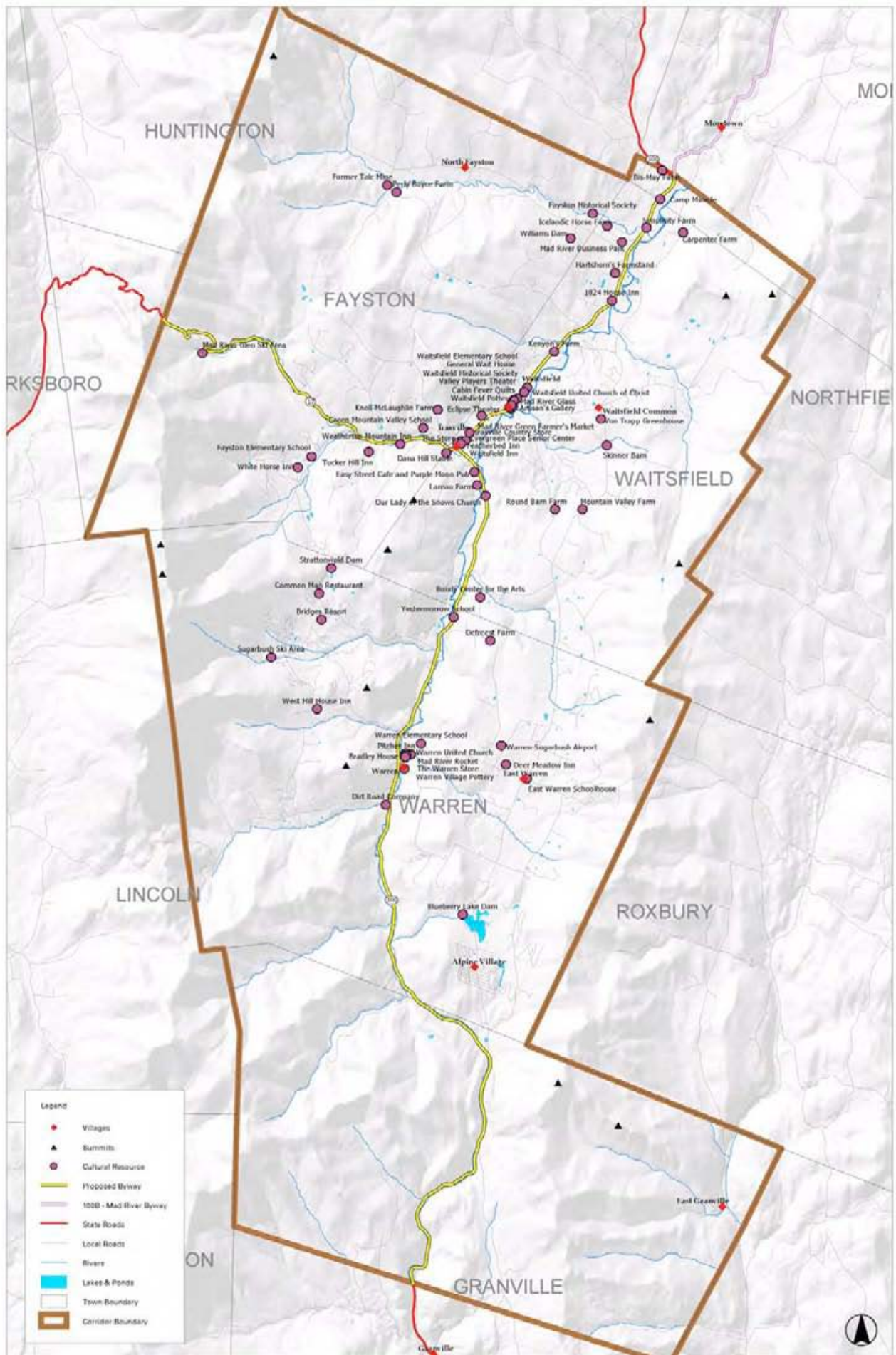
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RECREATIONAL QUALITIES MAP

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Call data provided by the local State Trading Company (SNC) and World Customs for comparison.

Prepared by: L. J. Matthews
 04/20/06
 04/20/06 4/21/06



Mad River Byway EXTENSION

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CULTURAL QUALITIES MAP

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Prepared by: Vermont Department of Transportation
 Date: 10/14/2014
 10/14/2014 10:14 AM
 10/14/2014 10:14 AM

Mad River Byway *EXTENSION*

DRAFT Nomination Package and Corridor Management Plan

ARCHAEOLOGICAL & HISTORIC QUALITIES MAP

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A. Byway Goals and Objectives

Five important goals are proposed for the Mad River Byway Extension. These local and regional goals provide the basis for identification, implementation, and management of the corridor's intrinsic resources and are intended to stimulate and promote economic, recreational, and environmental benefits for the corridor's communities and transportation systems. Most importantly, the goals supply the foundation for the long-term objectives and policies, which will be developed as part of the Corridor Management Plan. The five goals proposed for the Mad River Byway Extension include:

1. Multi-Modal Transportation

Goal: To provide reliable and harmonious multi-modal travel options.

Objective A.

Improve vehicle, pedestrian and bicycle mobility, activity, and safety within village centers and along the rural highway.

Policy A.1

Develop and implement sidewalk upgrade and connectivity plans.

Policy A.2

Widen variable shoulders along Routes 100 & 17 to accommodate bicycle and automobile traffic.

Policy A.3

Encourage use of the Mad River Pathway as a multimodal travel option.

Policy A.4

Identify conflicts with horse owners who ride along Routes 100 & 17 and develop alternative horse-friendly routes if conflicts are unavoidable.

Policy A.5

Identify and address conflicts with the island at the intersection of Routes 100 & 17, also improve Egan's and Mad River Glen parking.

IV. IMPLEMENTATION GOALS & OBJECTIVES

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Policy A.6

Monitor and address traffic safety concerns on Route 17 through Buel's Gore to Appalachian Gap in consultation with VTrans.

Objective B.

Implement and promote an integrated/linked transit system that allows commuter options as well as access into the region.

Policy B.1

Evaluate current transit systems and identify weaknesses and missing links.

Policy B.2

Implement localized public transportation system by 2016.

Policy B.3

Develop and appropriately site a public park and ride lot to provide easy access to transit options and commuter carpooling.

Policy B.4

Investigate usage of public school transportation for weekends and during the day.

Objective C.

Educate users of all ages on transportation options.

Policy C.1

Create a Mad River Valley Transportation website with bus route and stops, park and ride lots, carpool message board, etc.

Policy C.2

Bring programs to local schools to teach children about the benefits of multi-modal transportation.

2. Recreation

Goal: To improve access to recreational resources and facilitate collaborative approaches to recreational development and management.

IV. IMPLEMENTATION GOALS & OBJECTIVES

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Objective A.

Improve information, signs, and other wayfinding elements that direct visitors to various public and private recreation activities and destinations along the entire byway and increase traffic safety.

Policy A.1

Create a sign plan for public swimming holes along the byway.

Policy A.2

Create a sign plan for natural areas, including Granville Gulf.

Policy A.3

Coordinate all sign plans to be common to the Byway but unique to the resource.

Policy A.4

Explore additional traffic safety signage along Route 17 (formerly McCullough Turnpike), particularly in advance of sharp curves.

Policy A.5

Consider establishing an interpretive sign to present the history of McCullough's Turnpike and to highlight the dangerous conditions that are present along Route 17 through Appalachian Gap. The location for such a sign could be at the top of Appalachian Gap in one of the designated "pull out" parking areas.

Objective B.

Promote and improve connectivity of the Mad River Pathway.

Policy B.1

Offer private landowners incentives to cooperate with Pathway development through their properties and grant easements.

Policy B.2

Incorporate summer use of cross-country ski trails (e.g. Ole's network).

Policy B.3

Educate potential pathway users on the health benefits of walking and usability of the pathway.

IV. IMPLEMENTATION GOALS & OBJECTIVES

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Policy B.4

Build new trailheads near Meadow Road, Tremblay Road at Pines Rest Area and the General Wait House.

Objective C.

Continue to provide and care for public access sites along the Mad River.

Policy C.1

Continue to work with the Friends of the Mad River on streambank stabilization and other projects.

Policy C.2

Initiate public education campaigns on low-impact usage of the river at public sites.

3. Rural Character

Goal: To preserve and enhance the rural character and community values of the Byway Corridor.

Objective A.

Support villages in efforts to create a sense of community.

Policy A.1

Evaluate and develop plans for infill development to prevent sprawl.

Policy A.2

Support and encourage Main Street character and possible designation along Route 100.

Objective B.

Create a stronger awareness among residents and visitors of points of historic significance along the byway.

Policy B.1

Work with the town historical societies to preserve the stories and memories of the people behind the historic landscape and buildings.

Objective C.

Ensure that byway development does not harm traditional land uses.

IV. IMPLEMENTATION GOALS & OBJECTIVES

Draft for Review 9/11/07

Policy C.1

Develop incentive programs to keep historic farms operational.
Value added uses and open land.

Policy C.2

Preserve Warren Village's "town square feel."

Objective D.

Encourage visitors and residents to "stop, and get out of their cars" to appreciate town centers and rural character.

Policy D.1

Improve parking to be more centrally located with easy access to civic center businesses and offices.

Policy D.2

Slow down traffic, especially commercial, with lower speed limits and increased enforcement, and/or other traffic calming techniques.

4. Preservation

Goal: To plan for the preservation and interpretation of the unique scenic, historic, cultural, and natural resources found within the Byway Corridor.

Objective A.

Open space, agricultural lands, and natural habitats should be protected and preserved.

Policy A.1

Support local, regional, state, and private organizations' acquisition of land or conservation easements.

Policy A.2

Inform public of major wildlife crossings and work to protect and improve these areas by exploring options including animal-sized culverts and overpasses.

Policy A.3

Identify important wildlife habitat and create management plans to minimize human disturbance, especially along the roadway. Potential areas include: Rolston Road Ravine and Granville Gulf.

IV. IMPLEMENTATION GOALS & OBJECTIVES

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Objective B.

Create stronger awareness among residents and visitors of the unique natural resources that are protected in the valley.

Policy B.1

Encourage educational programs with local schools on protected public lands.

Policy B.2

Conduct an inventory of potential areas deserving preservation and encourage public participation in the process.

5. Promotion

Goal: To develop effective and integrated marketing and information strategies for Byway users and provide appropriate amenities for the traveling public.

Objective A.

Provide central locations for information distribution and local advertisement opportunities.

Policy A.1

Redevelop Top Gas property into tourist kiosk.

Policy A.2

Develop corresponding kiosk at northern entrance to Byway, e.g. General Wait House.

Objective B.

Develop a solid framework and strategic program for the byway that will market Routes 100 & 17 as a destination itself in addition to the numerous resources available to the public.

Policy B.1

Create and maintain a Mad River Byway Extension website that links to points of recreational, cultural, and historic interest along the route.

Policy B.2

Create and distribute bumper stickers to promote Byway objectives. Public suggestions include: "I'm a Mad River Byway Pace Car" and "I Brake for People."

IV. IMPLEMENTATION GOALS & OBJECTIVES

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Policy B.3

Coordinate efforts with existing Moretown kiosks.

Policy B.4

Develop a Sign Master Plan for all signs related to the Byway and its resources. Mad River Valley Chamber of Commerce supports use of their logo.

Objective C.

Alert Route 100 and 17 users of Vermont Byway status and significance.

Policy C.1

Create Byway Gateways with signage and/or other visual elements.

Policy C.2

Develop and implement creative signs at trestle bridge.

Objective D.

Expand Byway linkage to surrounding towns in the future.

Policy D.1

Encourage other towns along Routes 100 & 17 to seek Vermont Byway nomination.

IV. IMPLEMENTATION GOALS & OBJECTIVES

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A. Overview

As an integral part of the Mad River Byway Extension initiative, a number of specific projects have been identified as potential next steps for the development and enhancement of the Byway. The initial development and/or articulation of these projects as part of this Management Plan provides the basis to prepare an application for a second phase of funding for the implementation of the Byway program and its associated elements.

The Steering Committee, citizen participants and representatives of the Mad River Planning District have identified the Mad River Greenway project, which has been conceived and developed by the The Mad River Path Association, as a key component of the Valley's multi-modal travel options. The relationship of the Greenway to the Byway concept is direct: the Greenway has been conceived as a recreational trail system linking the towns and villages of the Mad River Valley, from Warren to Moretown. The Greenway route, in part, would follow or be part of the Byway corridor, and provide an alternative means of both recreational and, to a certain extent, functional travel alongside of the vehicular route provided by Route 100 itself.

The initial phase of work accomplished as part of the Byway Management Plan includes route delineation and design to plan for the ultimate in connectivity of the distinct existing sections of the Pathway System located within the corridor. The intent is also to recommend improvements and enhancements to key access points, such as the Meadow Road, Tremblay Road, Kingsbury Bridge, and General Wait House locations. These enhancements would include improved identification, access, parking and landscape elements.

A second project that has been identified is the need to develop a consistent signing and wayfinding system to provide better visibility of and access to the wide range of Valley amenities including recreational, cultural or historic destinations. Coupled with this effort would be the consideration of a graphic identity for the Byway itself, as there are now provisions within the state sign program, as administered by the Travel Information Council, for Byway Trailblazer signs and related wayfinding elements.

B. Project Priorities List and Phasing Options

The Byway Steering Committee has identified the following tasks as project priorities:

- Redevelop the former Top Gas station as a tourist kiosk, as well as developing an additional information plaza at the northern end of the Byway boundary, e.g. General Wait House.
- Improve safety, access and parking at trailheads along the corridor (e.g. near Meadow's Road, Tremblay Road at Pines Rest Area).
- Work with the State to identify and implement needed improvements at the Granville Gulf Reservation.
- Develop and implement a consistent and coordinated sign plan for public amenities and resources along the Byway.
- Improve the Route 100/17 intersection (e.g. put in an island, improve parking).
- Improve the parking situation at Mad River Glen to ensure safe access for vehicles and pedestrians
- Continue to expand and upgrade the Mad River Path/Greenway.

C. Sign Plan

A consistent family of wayfinding elements will serve to strengthen the Byway identity and should include roadside trailblazers signs, kiosks, trailhead signs and site identification signs.

Examples of Roadside trailblazers



Examples of Interpretive Signs





D. Path Implementation Matrix

Planning for pedestrian and bicycle alternatives has been an integral part of the work in developing the Mad River Byway Extension along Route 100. The Byway Planning Team, including representatives from LandWorks, the Central Vermont Regional Planning Commission, The Mad River Path Association and the Mad River Valley Planning District have worked to develop guidelines, details, options and pathway planning tools to further support the vision for the path in the Mad River Valley. That vision includes developing both a linear network parallel to the river and Route 100 running north south in the Valley from Moretown to Warren, as well as other connecting trails that access other destinations, resources and amenities within the Mad River Valley. This effort builds on the good work already accomplished by the Pathway Association in building the path network in the region. In fact, the Spring 2007 edition of the Association's newsletter highlights this effort in Executive Director Linda Lloyd's article on "The Mad River Valley Planning District and the Mad River Path" and states:

"Each year the Steering Committee sets out a Work Plan for the Executive Director. The first task on our list is working with the Mad River Path Association (MRPA) and the proposed Mad River Byway project. The MRPA has as a goal the completion of a walking greenway/recreational path from Warren to Moretown. The Route 100 Byways project seeks to designate Route 100 as a Byway from Moretown to Granville, and to improve pedestrian and bicycle mobility, activity and safety within the Village centers and along the rural highway, while encouraging use of the Mad River Path as a multi-modal travel option. In addition, the Byway Program would promote and improve connectivity of the Mad River Path. The MRVPD sees both projects as closely interrelated and the synergy of moving both projects forward as a top priority."

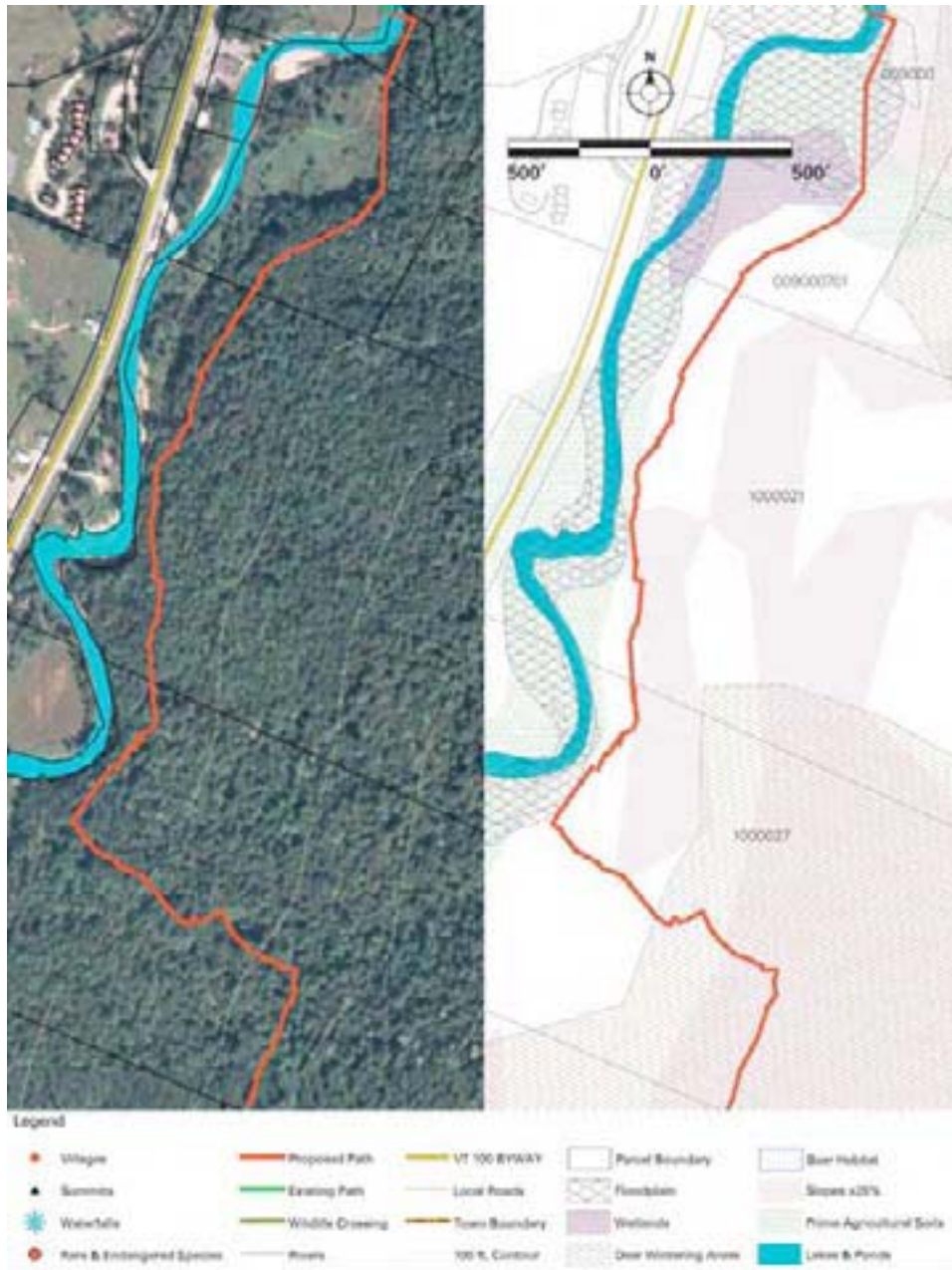
Clearly, as stated, the support of the Pathway work is directly related to the establishment of the Byway. These two initiatives work in concert to provide a transportation alternative to the busy Route 100 highway, to promote different modes of travel within the Byway Corridor, to further develop an important recreational amenity and community resource while at the same time providing access to important recreational and cultural destinations.

As part of the Byway planning, therefore, work was accomplished in

planning and development of the Pathway extensions, Pathway Trailheads and signing, as well as Pathway construction itself. The following tools and products have been produced in support of the Pathway component of the Mad River Byway Extension:

- 1) A Matrix planning tool that included mapping, environmental constraints, parcels and route descriptions/options;
- 2) A proposed pathway trailhead/connection design for the General Waite House in Waitsfield;
- 3) Mad River Path Construction Profiles;
- 4) Sign designs and wayfinding/information elements for the Pathway; and,
- 5) A Pathway Map of existing and proposed routes;

1) Sample Pages from the Path Implementation Matrix



SECTION BEGINS/ENDS	This section begins at the northern end of the existing Warren Path and will travel north to Riverside Park crossing the Mad River to join the existing Kingsbury Bridge Greenway.
LENGTH	1.01 miles
ROUTE DESCRIPTION	This route, as proposed, will pass through (4) four forested parcels, on a side hill slope down to the river. As proposed the route will travel between an open space on the west and woodland to the east in the most northern parcel.
STATUS	Proposed, Not constructed – Need to locate and construct route
PROPERTY STATUS	4 parcels, owners unknown; Status of availability and owner participation to be determined
ENVIRONMENTAL ISSUES	The proposed route is adjacent to the Mad River floodplain with the northern portion traveling through the floodplain. The northern portion of this section will also travel through prime agricultural soils. The southern portion of the route will travel through deer wintering habitat. The entire section is proposed on slopes greater than 25%.
CONSTRUCTION TYPE	Interim, Low Cost Path Establishment or Constructed Path. See accompanying Path Construction Alternative sketch.
NOTES	

2) Wait House Pathway Access

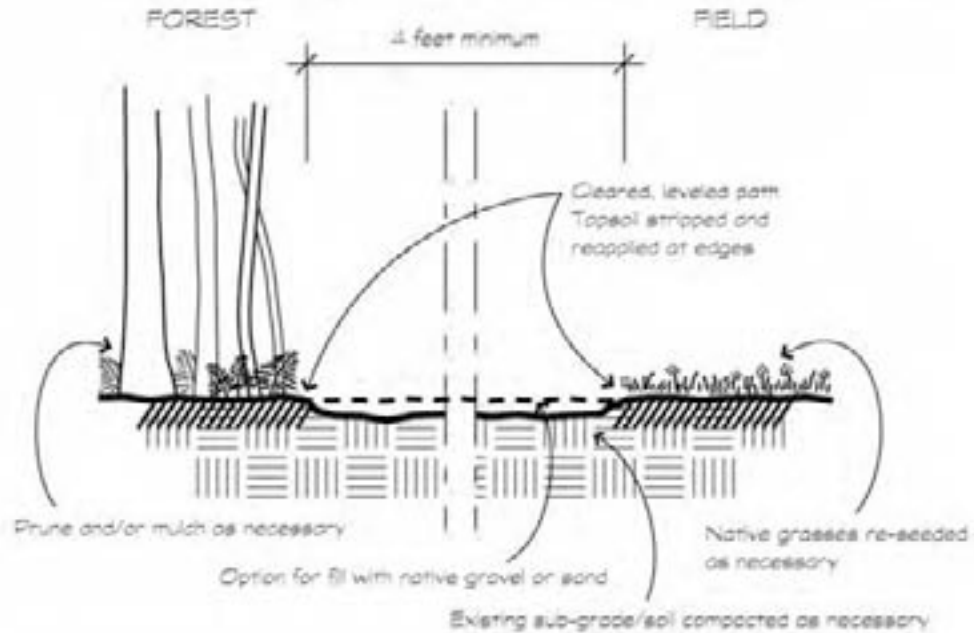
As one of the initiatives developed as part of the Route 100 Byway Planning, a new access point for a future segment of the Mad River Pathway has been proposed. This conceptual plan builds on previous planning and design efforts for the Wait House property and locates a new access for the pathway. The proposed route would then continue behind the elementary school to connect with an existing section of the Pathway. An information kiosk, benches and landscaping are proposed enhancements for this site and this segment of the path.



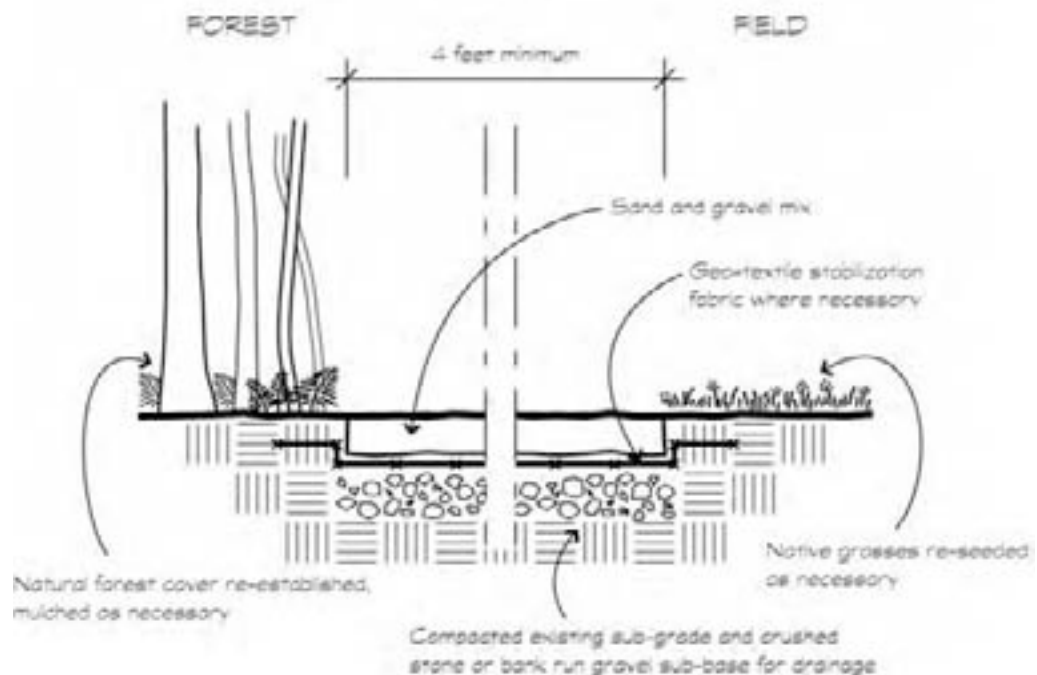
3) Mad River Path Construction Profiles

The following sketches illustrate potential construction methods for the Mad River Path and other recreational multi-use paths.

PATH CONSTRUCTION ALTERNATE A: Interim, Low Cost Path Establishment

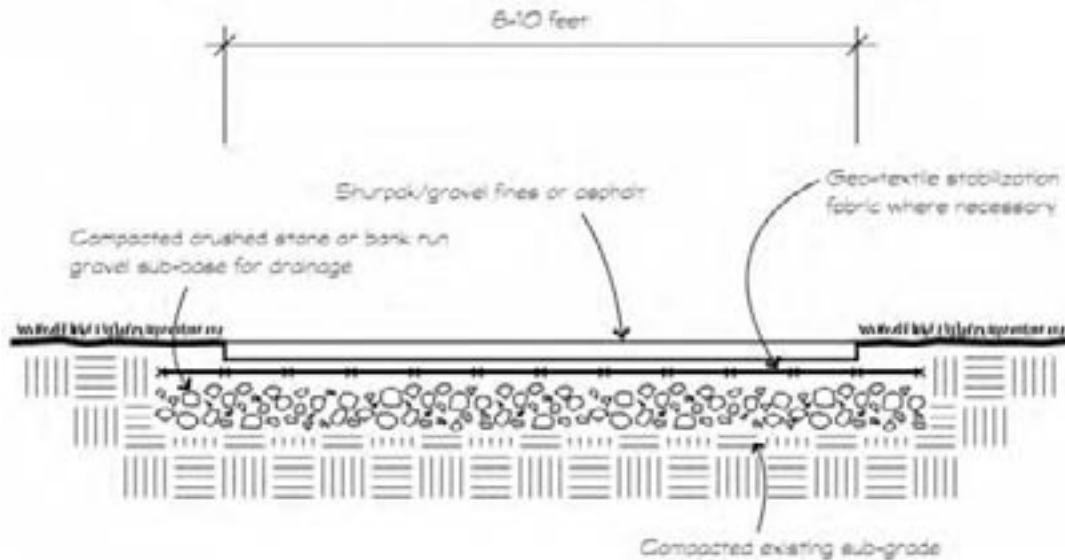


PATH CONSTRUCTION ALTERNATE B: Constructed Path

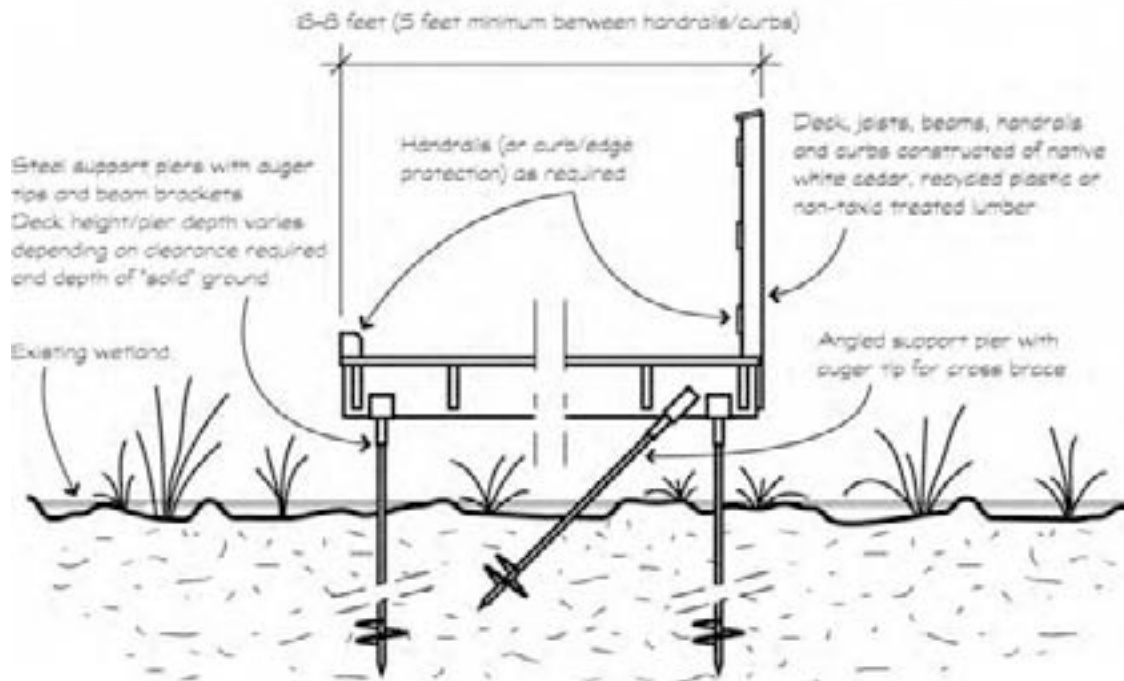


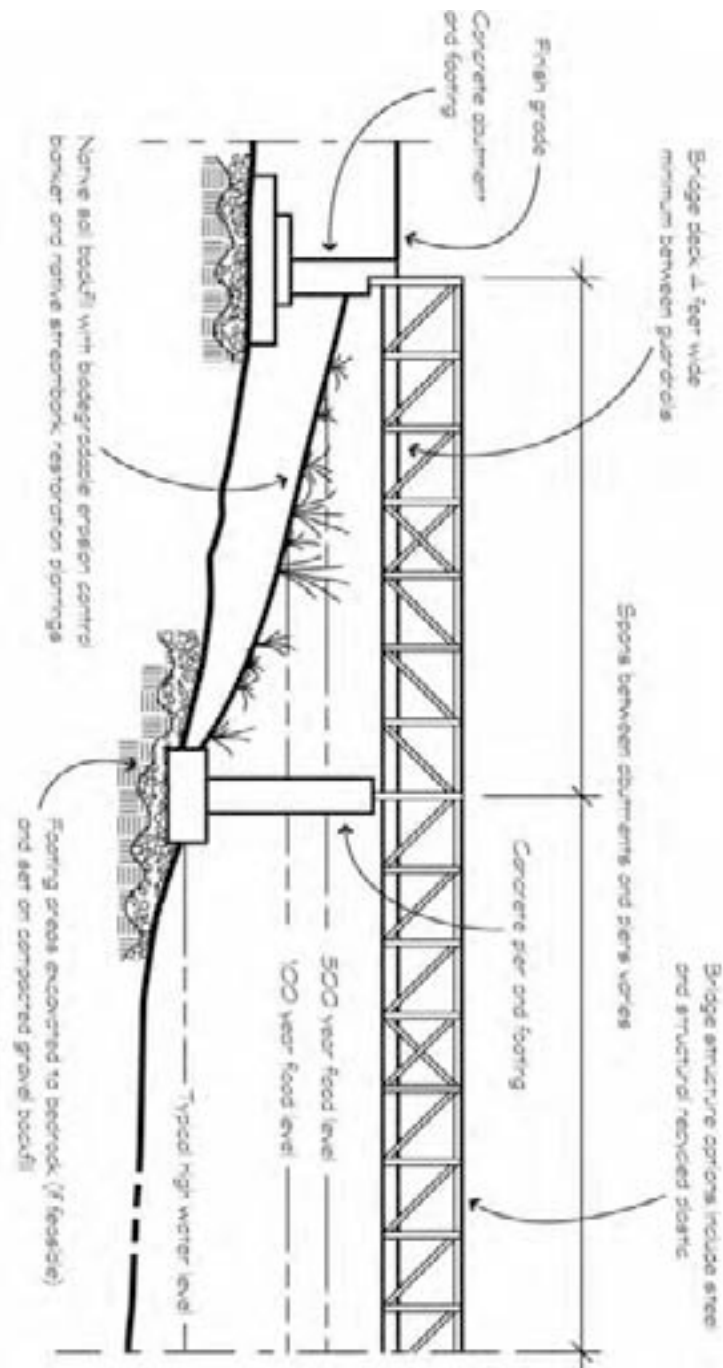
PATH CONSTRUCTION ALTERNATE C: "Widewalk" or "Wideway" Path

For future sections in more developed areas such as Inasville Growth Center, to serve pedestrians, skateboarders, rollerbladers, wheelchairs, etc.



PATH CONSTRUCTION ALTERNATE D: Wetland Boardwalk





PATH CONSTRUCTION ALTERNATE E: Bicycle and Pedestrian Bridge
For crossings of significant waterways

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4) Examples of Kiosks, Trailhead and Site Identification Signs

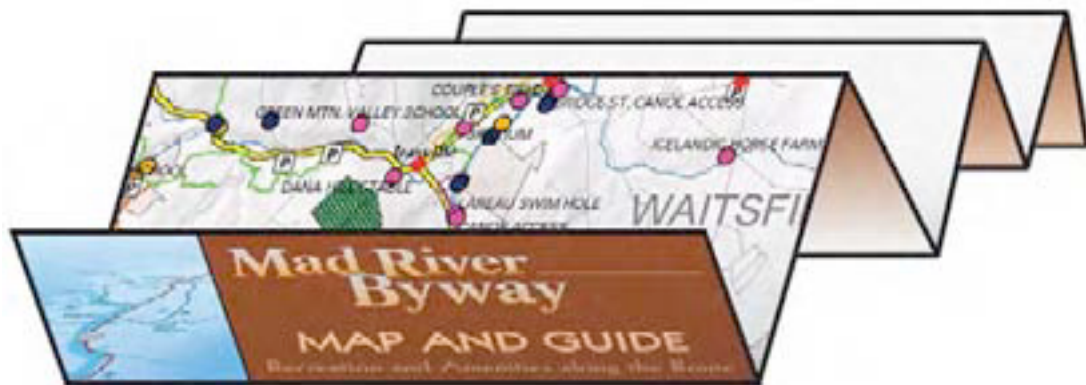


4) Pathway Map



E. Byway Brochure Concept

The following sketch illustrates a potential brochure/pamphlet concept. The format is conceived as an accordion folded piece that is sized to fit a standard brochure rack and can easily be refolded by a visitor to the section of the Byway they are currently exploring. The primary feature of the brochure would be a Byway map highlighting recreational opportunities, public amenities and services as well as other non-profit Byway resources and destinations. The reverse side of the brochure would present detailed information and photographs about the Byway and the many things to see and do along the route; these descriptions and images would correspond to the sites highlighted on the map.



MAD RIVER BYWAY EXTENSION

Transportation Management Plan

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INTRODUCTION

This report presents a transportation management plan for the proposed Mad River Byway Extension in Moretown, Fayston, Waitsfield, Warren, and a portion of Granville and Buels Gore, Vermont. The proposed byway is an extension of the recently designated Mad River Byway centered on VT 100B and the Mad River in Moretown.

The transportation assessment presented in this document is one component of a corridor management plan necessary to support designation of the rest of the Mad River Valley as a byway. A Vermont Byway is a highway or other public road that has special scenic, historic, recreational, cultural, archeological and/or natural qualities.

The Vermont Byways Program Manual recommends the following components related to transportation be included in the corridor management plan:

- A general review of the road's safety record to locate hazards and poor design, and identify possible corrections;
- A plan to accommodate commercial traffic while ensuring safety of bicyclists, joggers, pedestrians and sightseers in smaller vehicles; and
- Recommendations for modifying the roadway to support the corridor goals and to enhance the intrinsic qualities of the byway.

This report describes the general characteristics of the study area, summarizes recommendations made in previous studies and plans, describes the characteristics of the transportation system and its performance relative to congestion and safety, and summarizes a field assessment of access to recreation and cultural destinations in the corridor. Previous recommendations are combined with the ideas generated as part of this study into an overall transportation management plan for the proposed extension of the Mad River Byway.

STUDY AREA OVERVIEW

The study area is centered along VT 100 and also includes VT 17 as shown in Figure 1. The segment of VT 100 included is approximately 20 miles long from its intersection with VT 100B to the Granville Preserve. The roadway parallels the Mad River and passes through a mostly rural and wooded landscape in the towns of Moretown, Waitsfield, Warren, and Granville. VT 100 also passes just east of Fayston and provides access to that Town through several local roads and VT 17. There are three major village centers along the corridor which include Waitsfield Village and Irasville directly on VT 100 and Warren Village which is located on a local road just east of VT 100. Waitsfield Village and Warren Village include a mix of small scale commercial uses, municipal services, and homes. Irasville provides a higher concentration of commercial and retail uses. All three villages are important activity centers and destinations for residents and visitors to the Mad River Valley.



VT 17 climbs for approximately 6 miles from VT 100 to the Appalachian Gap. It passes through rugged terrain and forest land dotted with an occasional single family home. It provides direct access to the Mad River Glen ski area and is the only year round crossing of the Green Mountains between I-89 and VT 125 over Middlebury Gap. VT 17 is a popular route for cyclists, motorcyclists, and people touring by automobile. Many of these recreational travelers are destined for the parking area located at the Appalachian Gap. The Long Trailhead, a hiking trailhead from Massachusetts to Canada, also crosses at this location.

The Sugarbush Valley and Mad River Glenn ski areas are major recreational destinations that rely on the access provided by VT 100 and VT 17. Both roads provide direct access to the numerous recreational resources offered by the Mad River and surrounding mountains along the entire study area during all four seasons. These roads must also serve the daily travel needs of all residents, businesses, and travelers within the Mad River Valley.

Figure 1: Study Area



SUMMARY OF PREVIOUS TRANSPORTATION STUDIES AND PLANS

Central Vermont 2003 Regional Transportation Plan (Approved August 9, 2003)

The Central Vermont Region Transportation Plan establishes regional goals, identifies existing and future issues of regional significance, and recommends specific projects, services, and planning and design work to address those issues. The plan includes an extensive evaluation of existing and future travel demand, the physical condition of roads and bridges, congestion under existing and 2020 conditions along arterial highways and at major intersections, a safety analysis, and a comprehensive list of recommendations specific to the major transportation corridors in the region.

Table 1 lists the recommendations from the Region's Transportation Plan related to the study area.

Table 1: Summary of Recommendations from CVRPC 2003 Transportation Plan related to the Study Area

Category	Recommendation	Status
Transit	Develop public transit connection between Mad River Valley, Waterbury, Montpelier, and Burlington	
Transit	Extend Mad Bus – Valley Floor Shuttle to Moretown Village and Harwood Union High School.	
Transit	When demand warrants, increase frequency of the Mad Bus/Mad River Glen Shuttle and extend to the village.	
Pedestrian and Bicycle System	Construct the Mad River Greenway	
Pedestrian and Bicycle System	Designate a bicycle route along VT 17 between VT 100 and German Flats Road	
Intersection	VT 100-Warren Main Street. Reconfigure the “Y” intersection to a “T”	The Town of Warren has expressed its desire to keep the existing configuration “as is”
Intersection	VT 100-Sugurbush Access Road. Long-term strategy is to replace the bridge and reconstruct the roadway. Short-term recommendation is to provide a traffic control officer during peak periods.	
Bridge	Kingsbury Bridge is under design for replacement. Town has requested a new truss bridge. VTrans has resisted due to cost. A bicycle and pedestrian underpass will be included in the reconstruction.	
Bridge	A bridge south of Irasville is classified as functionally deficient	
Bridge	The bridge over Mill Brook, just west of VT 100 is structurally deficient and needs to be replaced	Delayed by the disagreement over the VT 100/VT 17 intersection.
Intersection	VT 100-VT 17. LOS F projected for 2020. In conjunction with a VT 17 bridge over Mill Brook, VTrans is considering intersection design alternatives.	Disagreement over the preferred alternative has stalled any improvements from moving forward.
Roadway	Turn lanes should be considered to the shopping areas along VT 100 between VT 17 and Waitsfield Village	



Pedestrians and Bikes	Sidewalks, crosswalks, traffic calming and gateways should be provided along VT 100 between Irasville and Waitsfield Village	VT 100 Transportation Path is moving forward through design. CVRPC and MRVPD recently completed a study that recommends additional sidewalks and crosswalks in Waitsfield Village
Intermodal	Consider constructing park and ride lots near the VT 100-VT 100B intersection and in Waitsfield Village	

VT 100-VT 17 Scoping Study (2000)

An initial scoping report was completed in 1999¹ to develop and evaluate alternative designs at the VT 100-VT 17 intersection and to replace the bridge over Mill Brook just west of the intersection on VT 17. The initial scoping report evaluated five different alternatives. An amendment was completed in 2000² to provide additional information on the fifth alternative.

The purpose of the bridge project is to provide a safe crossing for VT 17 over Mill Brook while improving the adjacent roadway approaches, in conjunction with making improvements to the VT 100-VT 17 intersection, while preserving the scenic qualities of the surrounding area. The bridge's specific deficiencies include its alignment on a sharp curve, inadequate width (per state and federal standards) and load bearing capacity.

The purpose of the VT 100-VT 17 intersection project is to enhance mobility, improve safety and alleviate traffic operational problems, in conjunction with the bridge project. Specific issues include the intersection's "Y" configuration which allows two-way traffic on all approaches and increases the number of conflict points, its location on a steep grade and sharp curve, crashes associated with the "Y" configuration, and substantial delay for vehicles turning left from VT 17 to VT 100.

The alternatives were narrowed down to:

Alternative 2A – Replace the bridge on its existing alignment. Reconfigure the VT 100-VT 17 intersection to a "T" with a right-turn slip lane from the VT 100 southbound approach to VT 17. VT 17 would be controlled with a stop sign. Estimated cost: \$1.51 million.

Alternative 5 – Remove the existing bridge (or keep in place for bicycle and pedestrian use only). Build a new VT 17 alignment west of Mill Brook to intersect VT 100 approximately 1640 feet south of the existing intersection. This alignment would eliminate the need for a bridge on VT 17. The new

¹ "Fayston-Waitsfield RSRGC-RS0200(7) VT Route 17 over Mill Brook (Bridge No. 38) VT Route 17/VT Route 100 Intersection Initial Scoping Report"; Dufresne-Henry for the State of Vermont Agency of Transportation; December 1999.

² "Fayston-Waitsfield RSRGC-RS0200(7) VT Route 17 over Mill Brook (Bridge No. 38) VT Route 17/VT Route 100 Intersection Initial Technical Report: Alternative 5 Assessment"; Dufresne-Henry for the State of Vermont Agency of Transportation; December 5, 2000.



intersection with VT 100 would be either a “T” or a roundabout. Estimated cost: \$2.6 to over \$3.0 million.

A locally preferred alternative has not been selected for this project. The Town of Waitsfield has expressed an interest to address existing access management issues around the intersection in the short-term. The access management issues include poorly defined parking areas, particularly on the north-west corner of the intersection.

VT 100 Transportation Path (Ongoing since 1993)

In 1993, the Vermont Agency of Transportation selected the VT 100 Transportation Path for funding through the Statewide Bicycle and Pedestrian Program. The intent of the VT 100 Transportation Path is to provide pedestrian and bicycle connections between the villages of Waitsfield and Irasville.

The identified pathway runs from Bragg Hill Road in Irasville north along VT 100 to the Waitsfield Elementary School. The Transportation Path will include a five foot wide sidewalk along one side of the road, four foot marked bicycle lanes along both sides of the road, granite curbing, enhanced landscaping and lighting, and new crosswalks across VT 100. Additional enhancements identified for development in conjunction with the pathway project include the following:

- Construct a gravel pathway connecting the Irasville Common;
- Upgrading the Village Path;
- Sidewalk improvements on the west side of Waitsfield Village;
- Sidewalk improvements on Bridge Street; and
- New sidewalks with development in the growth center of Irasville.

As of the date of this report, the design work for the path is substantially complete. The project is listed in the development/evaluation section of the draft 2007 VTrans Capital Program indicating that construction may be funded within a few years.

Waitsfield, Warren, Fayston Bicycle Facility Feasibility Studies and Recommendations (1996)¹

Three studies were completed in 1996 that evaluate the feasibility of improving specific segments along or near VT 100 in Waitsfield, Warren, and Fayston, and along VT 17, with the overall goal of developing a coordinated bikeway system through the three towns.

Segment A: Waitsfield Northern Connector. Extends between the Waitsfield Elementary School (and the northern end of the VT 100 Transportation Path) to a trailhead head for the Mad River

¹ “Waitsfield Transportation Path WFD-94-035 Conceptual Alignment Analysis”; Warren Northern Connector WRN-94-036 Conceptual Alignment Analysis”; “Fayston Transportation Path Conceptual Alignment”; The Boyle/Kennedy Association; April 19, 1996.



Greenway at Tremblay Road. An on-road bicycle facility is recommended. The recommended cross-section consists of ten-foot wide travel lanes and five-foot wide bicycle lanes with appropriate pavement markings and signs. Estimated cost: \$43,000 (1995 dollars).

Segment B: Warren/Waitsfield Northern Connector. Extends from Irasville to the Warren Town Line (and the northern end of the Warren Northern Connector). An on-road bicycle facility is recommended. The recommended cross-section consists of eleven-foot wide travel lanes and four-foot wide bicycle lanes with appropriate pavement markings and signs. Estimated cost: \$15,000 (1995 dollars).

Warren Northern Connector: Extends from the Warren/Waitsfield town line to the Warren Elementary School in Warren Village. An on-road bicycle facility is recommended. The recommended cross-section consists of eleven-foot wide travel lanes and four-foot wide bicycle lanes with appropriate pavement markings and signs. This study notes the constraint caused by the Kingsbury Bridge. If the bridge is reconstructed, this study recommends its cross-section include eleven-foot wide travel lanes and four-foot wide bike lanes. Estimated cost: \$5,800 (1995 dollars, not including bridge)

Segment C: Fayston/Waitsfield Connector at VT 100-VT 17 Intersection. This facility would provide the final connection between the proposed Fayston Transportation Path (which follows VT 17) to VT 100. It assumes that Alternative 5 of the VT 100-VT 17 intersection is reconstructed. Therefore, the VT 17 bridge over Mill Brook would be available for exclusive use as a bicycle and pedestrian bridge. A short, ten-foot wide bicycle path is proposed from the eastern end of the bridge to VT 100. Because Alternative 5 at the VT 100-VT 17 intersection may not be built, this recommendation is less certain. Estimated cost: \$23,500 (1996 dollars).

Fayston Transportation Path: Connects the Fayston Elementary School on German Flats Road to the proposed on-road bicycle facilities along VT 100 as described above. The study acknowledges that little can be done to change the physical character of VT 17 that would not adversely affect the highway character and landowners. The recommended improvement is to install “Share the Road-Mad River Greenway” signs on both directions. Estimated cost: \$1,800 (1996 dollars).

The Warren Village Pedestrian Enhancement Plan (2005)¹

This plan evaluated the feasibility of developing pedestrian facilities, landscaping enhancements, traffic calming elements and related amenities in the Village of Warren along Main Street. A sidewalk is proposed on the west side of Main Street starting across from Fuller Hill Road to the bridge over Freeman Brook just south of the Warren Store. Textured crosswalks are recommended at several locations, speed tables are proposed at entrances to the Village, and lighting and benches are included. Estimated cost: \$201,380 (2004 dollars).

¹ “The Warren Village Pedestrian Enhancement Plan, Feasibility Report Draft”; LandWorks for the Town of Warren; July 14, 2004.



Waitsfield Village Parking and Pedestrian Circulation Study (2006)

This study recommends sidewalk, access management, traffic calming, parking, and enhancements in Waitsfield Village. The study area is located in Waitsfield Village along VT 100 and Bridge Street. It is anchored by the VT 100-Bridge Street intersection in the south and extends approximately 0.40 miles to the southern most intersection of VT 100 with Old County Road.

Recommendations for Implementation within 5 years:

- Farr Lane to Valley Players Theatre. Construct a new sidewalk, green strip, and on-street parking on the west side of VT 100. Improve driveway definition and include street trees where possible and pedestrian scale lighting. The recommended concept plan is coordinated with the cross-section proposed for the VT 100 Transportation Path which includes upgrades to the existing sidewalk and on-street parking on the east side of VT 100, and bike lanes on both sides of the roadway. Estimated Cost: \$494,000.
- Mid-block pedestrian crossings with bulbouts on VT 100 just north of the entrance/exit to the Bridge Street Market Place lot; at the Village Grocery; and near the Valley Players Theatre, and Old County Road. Estimated cost: \$47,000.
- VT 100-Bridge Street Intersection. Add curb extensions, raised pedestrian crossings, and improve the definition of Farr Lane. Estimated Cost \$99,000.

Recommendations for Implementation within 5-10 years:

- Valley Players Theatre to Old County Road. Construct a new sidewalk, green strip, and on-street parking on the west side of VT 100 with street trees and pedestrian scale lighting. Estimated cost \$376,000.
- VT 100-Bridge Street. Install a traffic signal. Estimated cost: \$175,000.
- Place utilities underground. Estimated costs: \$1.6-3.6 million.
- The Town should purchase the Bridge Street Marketplace parking lot to allow implementation of a plan that reorganizes the lot, provides an opportunity to create a riverside park in the Village, and helps improve local circulation. Estimated costs: \$357,000.

1997 Waitsfield Circulation and Access Management Plan¹

The 2006 Waitsfield Study incorporates and refines many of the recommendations contained in this study. This study calls for the expansion of new local roads parallel to VT 100.

¹ Prepared by Lamoureux, Stone, and O'Leary for the Town of Waitsfield and the Central Vermont Regional Planning Commission; October 17, 2006.



TRANSPORTATION SYSTEM CHARACTERISTICS AND PERFORMANCE

HIGHWAY SYSTEM CONTEXT

The roadways and intersections in the study area are components of a connected local, state, and national highway network. This section provides background information on these different systems and their relevance in the study area.

Highway functional class, the National Highway System, the Vermont Truck Network and town highway classification are the foundation for a variety of policies that affect funding eligibility, project prioritization, design requirements, jurisdiction, and maintenance and operation responsibilities for a highway. These various classification systems also provide a big picture view that defines the function of a specific highway within the context of the regional, state, and national transportation systems.

Functional Class

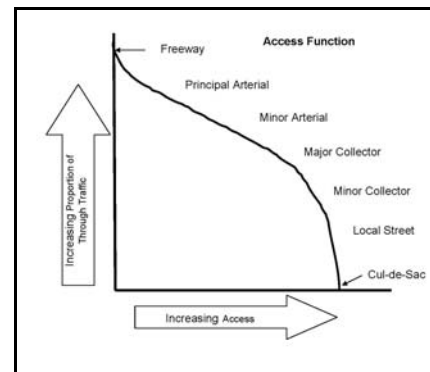
The highway functional classification system, depicted in Figure 2, is organized as a hierarchy of facilities, based on the degree to which the roadway facility serves mobility (through traffic) and access to adjacent land uses. Interstate highways, at the top of the hierarchy, are devoted exclusively to mobility, with no direct access to adjacent land. Arterials and collectors provide both mobility and access. The local road system is devoted exclusively to providing local access, with limited capacity and relatively slow speeds.

As shown in Figure 3 on page 11, the study area contains a specialized road network that consists of highways classified as minor arterials, major collectors, minor collectors, and local streets. VT100 is classified as a rural minor arterial throughout the study area. VT 100B and VT 17 are classified as rural major collectors throughout the study area.

As a minor arterial, the official function of VT 100 is to provide for longer trips at higher speeds. This function is challenged as VT 100 passes through Waitsfield Village and Irasville, where the roadway also provides direct access to adjacent properties and also serves a higher level of pedestrian activity. As noted above, plans have been completed for Waitsfield Village and Irasville to help calm traffic as it enters these activity centers, to improve access for pedestrians and cyclists, and to enhance the aesthetics and quality of life for residents, employees, and visitors.

The function of VT 100 as a minor arterial may also be at odds with its role of providing direct access to numerous recreational sites throughout the study roads. Many of these sites are located along the rural sections of the highway where motorists expect to travel at higher speeds. To maintain a reasonable level of mobility for through traffic while providing for safe and efficient

Figure 2. Conceptual Roadway Functional Hierarchy



access to recreational resources, left turn lanes may be warranted at some of the more heavily used destinations.

VT 17 is classified as a major collector. It should provide an equal balance of mobility for through traffic and access to adjacent land and streets.

Jurisdiction

VT 100 and VT 17 in the study are under the jurisdiction of VTrans. VTrans is responsible for all snow plowing, general maintenance, resurfacing, and reconstruction of the highway. Although VTrans owns and controls the roadway, decisions over design and reconstruction are made through a collaborative and public process that involves municipalities, the regional planning commission, and citizens.

Access Management Class

Because both roads are owned by the state, VTrans is responsible for issuing access permits on VT 100 and VT 17. VTrans has established an Access Management Program that assigns all segments of the State's Highway System into one of six access management categories. The standards provide the basis for access permitting on state highways and are used in the planning and development of VTrans roadway construction projects. Existing highways are not required to meet the design standards. However, the standards are applied to all new access permits and construction projects.

The access management categories, which are summarized in Table 2, specify whether or not direct access to adjacent property is permitted, the type of driveway design factors to be considered, and type of turning movement allowed (Traffic Operations).

Table 2. VTrans Access Management Categories

Access Category	Functional Class and AADT Characteristics	Direct Property Access	Driveway Design Factors	Traffic Operations and Movements Allowed	Design Features
1	- Interstates	No	Not Applicable	Access only provided at Interchanges with public highways	Grade-Separated Interchanges
2	- Other Principal Arterials - Limited Access Major Collectors	No – Except by Access Rights	Number, Spacing and Locations	Access at intersections with public highways	At-Grade or Grade-Separated intersections at ½ to 1 mile intervals
3	- Other Principal Arterials - Minor Arterials (AADT > 5,000) - Non-limited Access Major Collectors on State Highway and Class 1 Town Highways (AADT greater than 5,000)	Deny, Restrict or Allow	Number, Spacing and Locations	May limit turning movements	- Physical Barriers (Medians or Islands) - Traffic signal spacing requirements - Left and/or Right Turn Lanes Required - Spacing of public highway intersections that are or may be signalized (1/4 to ½ mile)
4	- Minor Collectors - Minor Arterials and Class 1 Town Highways (< 5,000 AADT) - Non-limited Access Major Collectors on State Highway and Class 1 Town Highways (Less than 5,000 AADT)	Yes	Number, Spacing and Locations	All turns in & out May limit turning movements	- Spacing of public highway intersections that are or may be signalized (1/4 to ½ mile)
5	- Frontage or Service Road	Yes	Number and location	All turns in and out	- Traffic signal spacing not less than 500 feet.
6	- May have any functional class but are urban in nature.	Deny, restrict, or allow	Number and location		- Traffic signal spacing not less than 500 feet.



Within the study area, VT 100 and VT 17 are designated in access categories 3, 4, and 6. In general VTrans can limit access to VT 100 or VT 17 to one access point per parcel for all of the access management categories in the study area.

As seen in Figure 4 on page 12, the section of VT 100 north of Waitsfield Village to the VT 100-VT 100B intersection is classified as category 3. VTrans can prohibit direct access to VT 100 from adjacent parcels on this segment of roadway if safe and efficient access is available on a side street. This policy should be implemented, where feasible, for access to a recreational or cultural resource along the byway.

The remaining sections of VT 100 outside of the villages, and all of VT 17, are classified as category 4. VTrans can not prohibit direct access from adjacent parcels on these roadway segments, but guidelines regarding driveway design and location still apply.

The category 6 sections of VT 100 are in village areas. Category 6 and Category 3 are similar in that VTrans can deny direct access to the highway if reasonable and safe access is available on a side street. Although not required by VTrans, access in village centers can be enhanced by locating shared parking behind blocks or clusters of buildings. One or two access points would be provided to the parking lot which would be connected to the rest of the village with sidewalks or other pedestrian facilities. Examples of this approach exist in Irasville and Waitsfield Village.



Figure 3. Highway Functional Classes

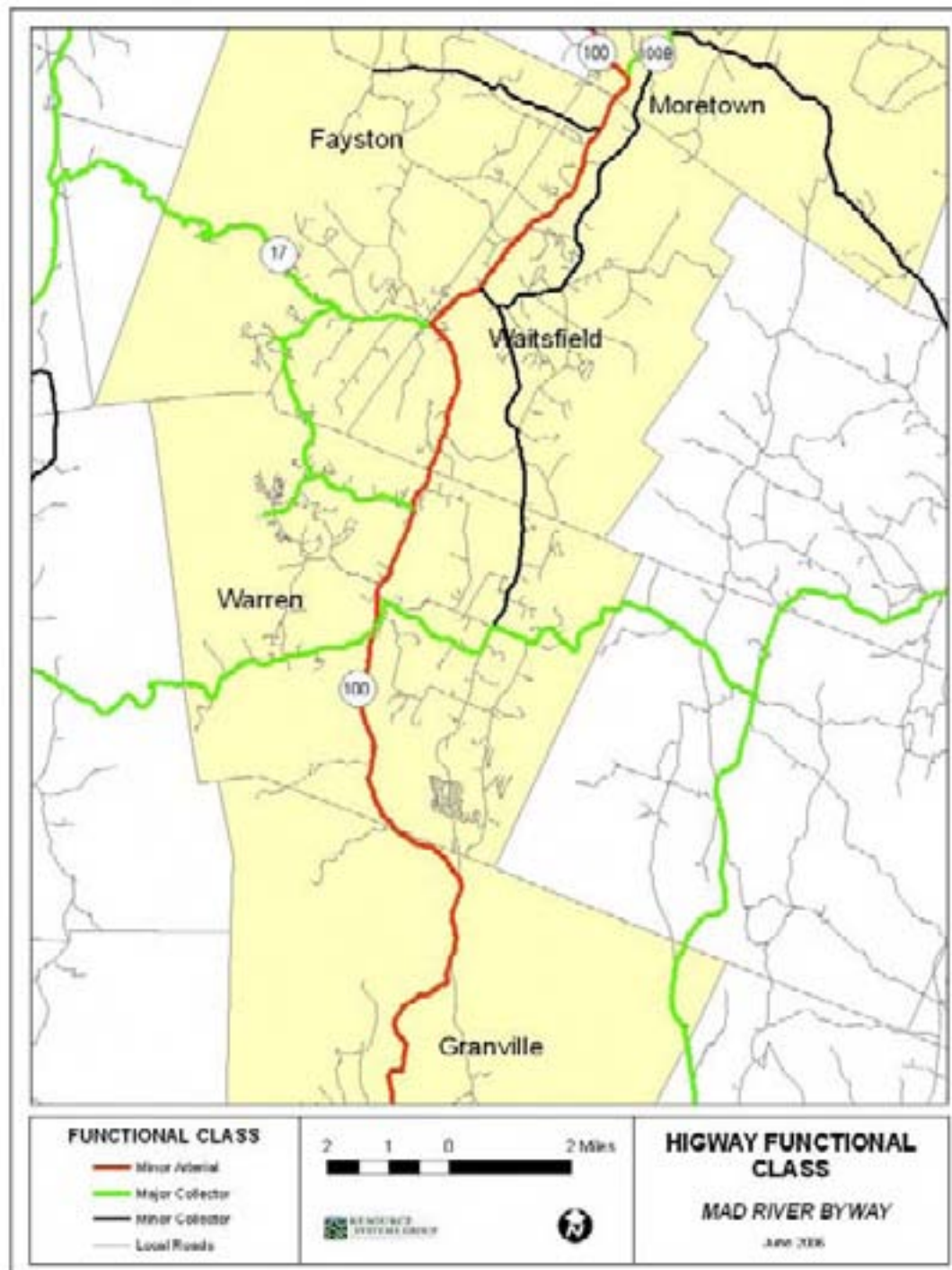
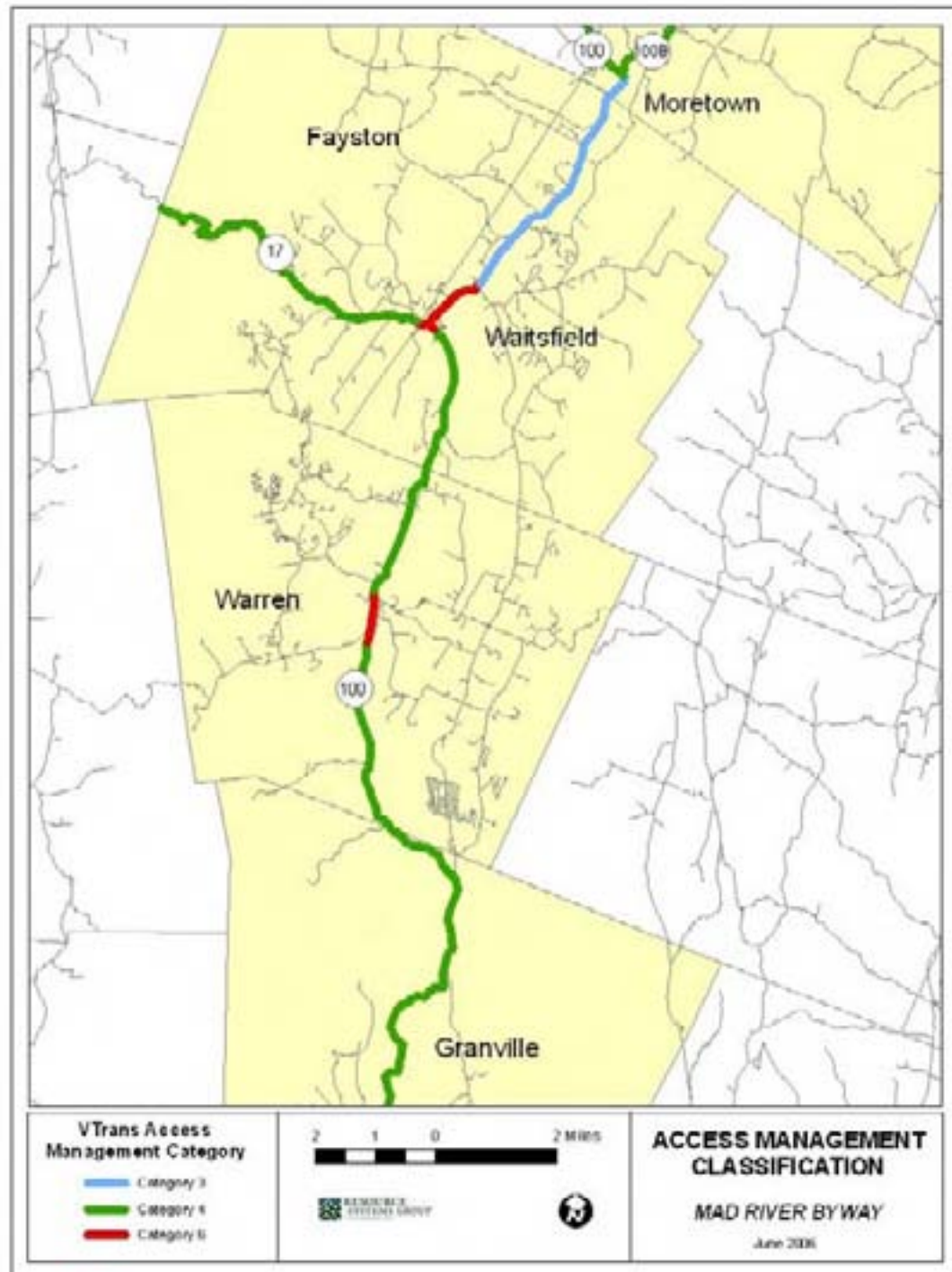


Figure 4: VTrans Access Management Categories



ROADWAY CHARACTERISTICS

Figure 5 shows the typical cross-section for VT 100. Exceptions include the segment between North Fayston Road and VT 100B with travel lanes that vary from 11 feet to 10.5 feet, and the segment through Waitsfield Village, shown in Figure 6. The typical cross-section for VT 17 consists of 12 foot wide travel lanes and 1-2 foot wide shoulders.

Figure 5: Typical VT 100 Rural Section

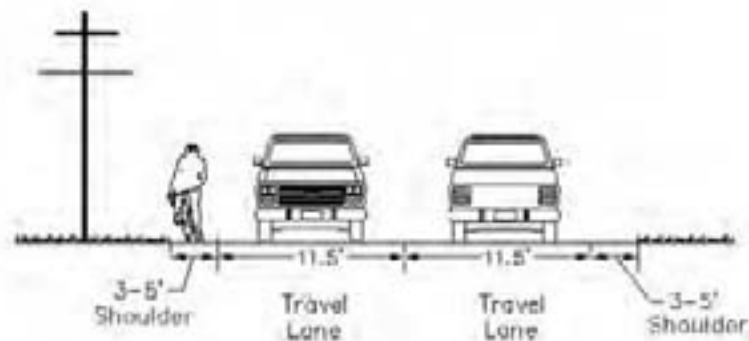
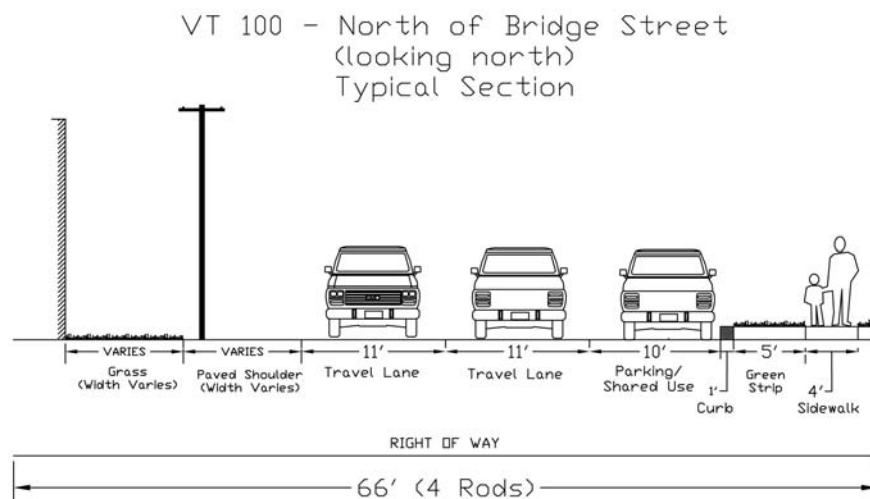


Figure 6: VT 100 through Waitsfield Village



VT 100 follows the Mad River and VT 17 climbs from the river valley to a mountain pass. As a result, the alignment of each road consists of numerous curves. As noted in the Vermont State Design Standards, horizontal curvature will normally be designed in accordance with AASHTO values for a particular design speed. Figure 7 shows the location of roadway curves with radii that do not satisfy AASHTO design guidelines.



The guidelines are applied when a section of roadway is reconstructed (they do not apply for a paving project which stays on-alignment). The Vermont State Design Standards provide flexibility in application of these guidelines. It allows smaller radii (sharper curves), where necessary to avoid and/or minimize disturbance of historic, archeological, scenic, natural, or other resources. The winding road nature of both roadways contributes to the character of the proposed byway. If and when sections are reconstructed and redesigned, the existing alignment should be preserved, and as noted in the Design Standards, advisory speed limits posted where appropriate.

Speed limits in the study area are shown in Figure 8. VT 100 generally has a speed limit of 50 miles per hour, with the exception of Irasville and Waitsfield Village which have speed limits varying between 30 and 40 miles per hour. VT 17 has a speed limit of 40 miles per hour which drops to 30 miles per hour near Irasville.

The speed of vehicles traveling on arterial highways through village centers is a common issue in Vermont. In the proposed extension of the Mad River Byway, the issue is of particular concern in Waitsfield Village and Irasville, which are located directly on VT 100. The Waitsfield Village Parking and Pedestrian Circulation Study (2006) includes a traffic calming plan for the Village that consists of gateways, bulbouts and curb extensions, on-street parking, and raised pedestrian crossings. These recommendations are necessary to preserve and enhance the quality of life, aesthetics, and economic vitality of the Village and should be included in the management plan for the byway.



Figure 7: Roadway Alignment

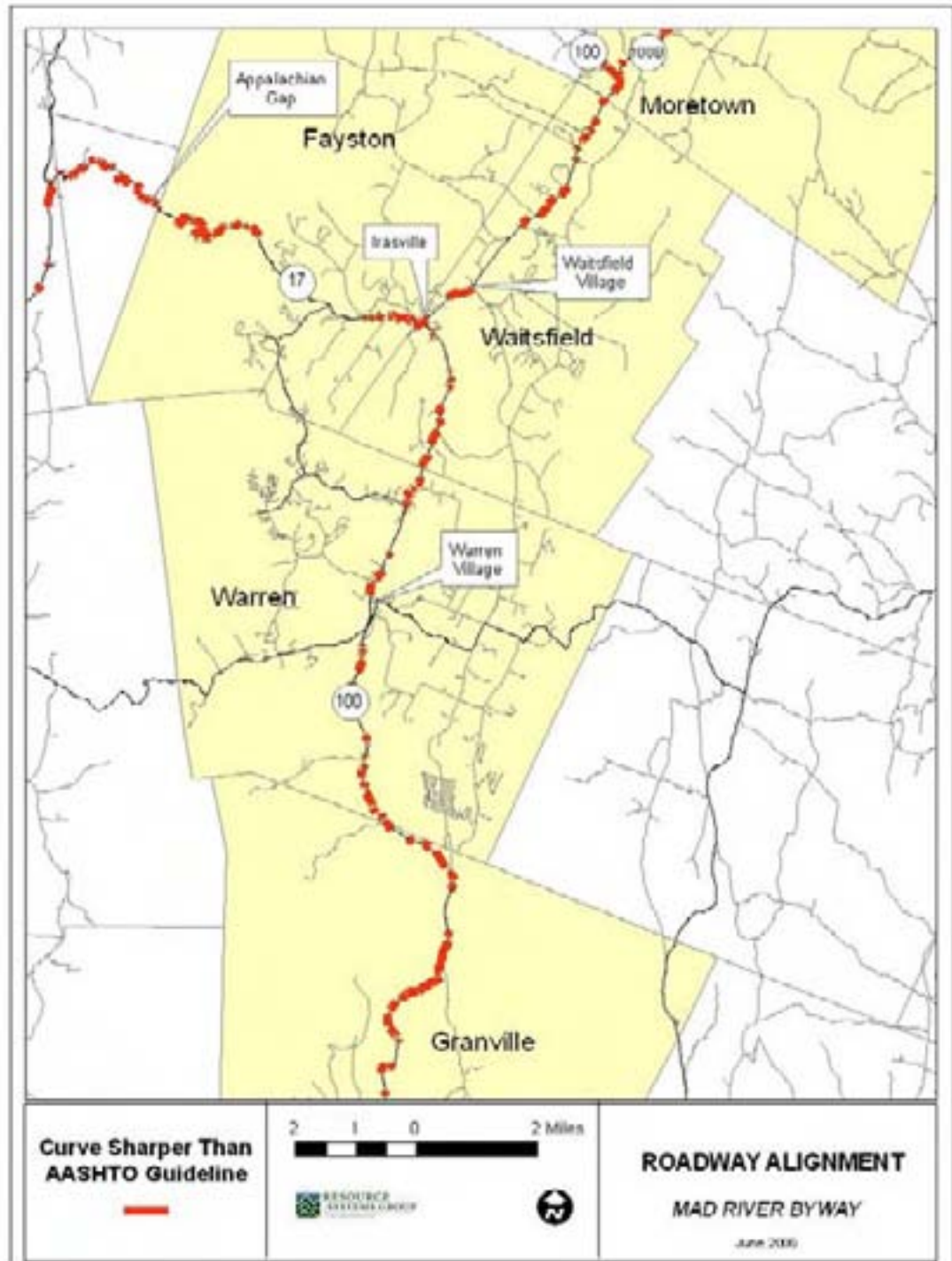
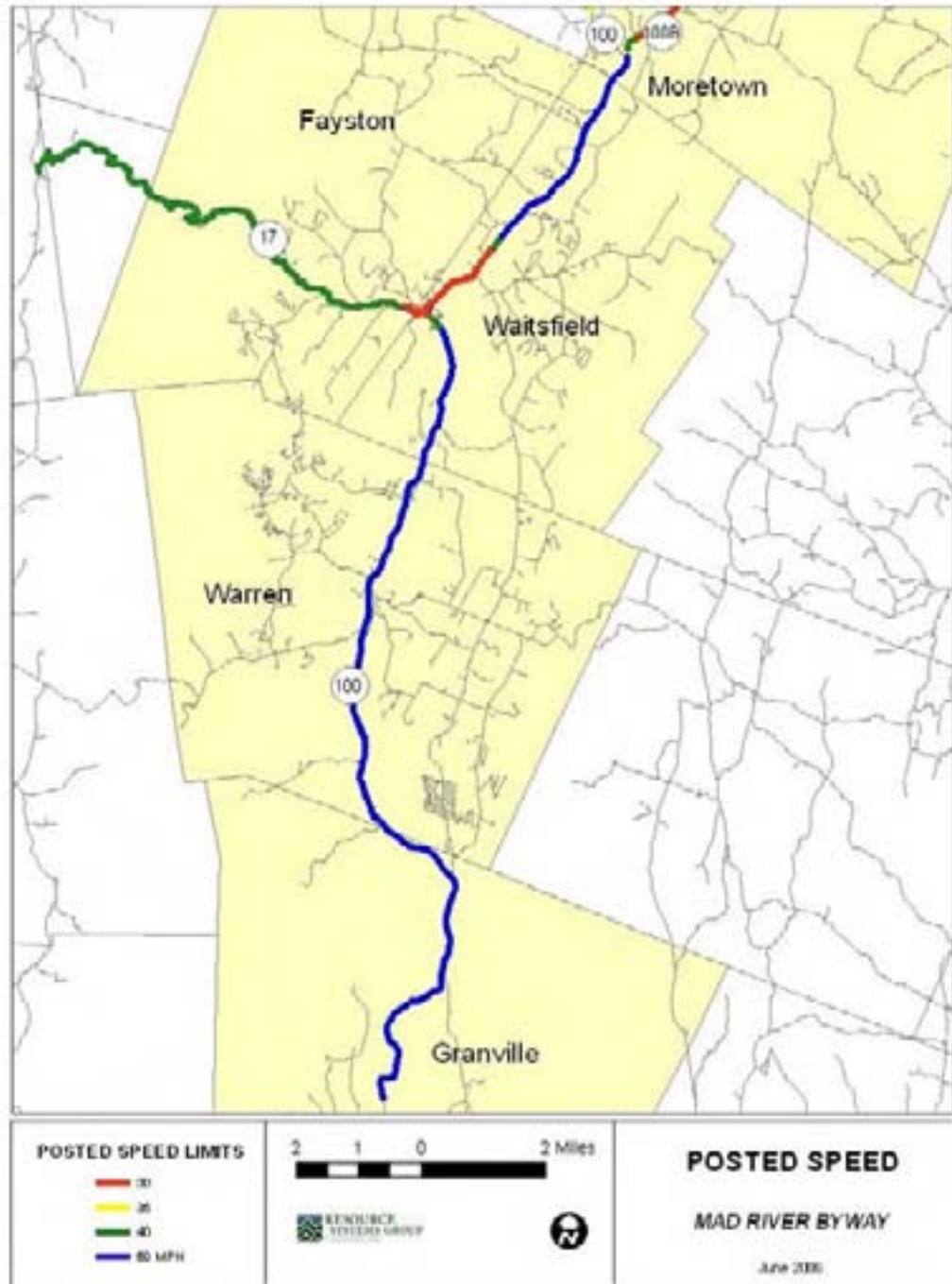


Figure 8: Posted Speed Limits



PEDESTRIAN AND BICYCLE FACILITIES

Pedestrian Travel

Existing sidewalks are limited to Waitsfield Village, Irasville, and Warren Village. Figure 9 shows the existing pedestrian network in Waitsfield Village. In addition to the sidewalks along each side of Bridge Street and the east side of VT 100, Waitsfield's pedestrian network consists of several unique features:

- Alleys that connect the Bridge Street Market Place parking lot to Bridge Street;
- A stairway from the Bridge Street Market Place parking lot to VT 100;
- The covered walkway along the Bridge Street Marketplace buildings that front VT 100;
- The walkway in the covered Bridge; and
- The walkway from Bridge Street through the park to the Waitsfield Church.

These features create pleasant and interesting spaces for pedestrians and therefore encourage walking. They support village vitality by providing places for people to interact and places for special events such as sidewalk sales and flea markets.

The Waitsfield Village Parking and Pedestrian Circulation Study incorporates these concepts. It recommends a new sidewalk on the west of VT 100; a pocket park in front of the Valley Players Theatre; additional crosswalks at the Village Grocery, Valley Players Theatre, and Old County Road; and enhancements to the green adjacent to the General Wait House. All of these improvements are intended to encourage walking by providing a safe, interesting, and pleasant environment for pedestrians.

The existing sidewalk extends on the east side of VT 100 from the General Wait House to the Slow Road in Irasville. This section of sidewalk will be replaced by the VT 100 Transportation Path, which will extend from Bragg Hill Road in Irasville north along VT 100 to the Waitsfield Elementary School.

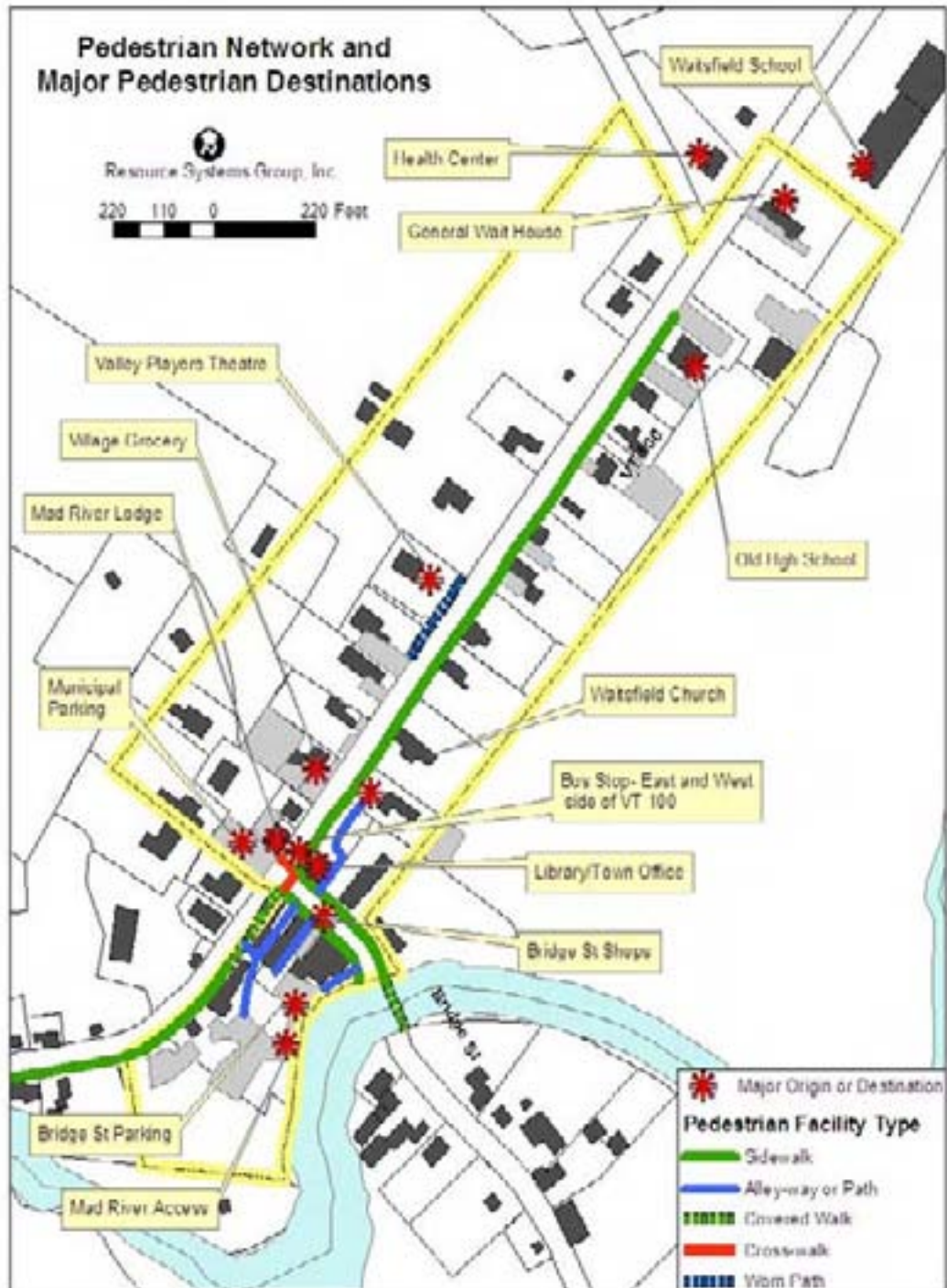
Pedestrian circulation in Irasville is also provided for on the Village Path. As described in the Map & Guide to trailheads in the Mad River Valley "...the path travels along the Mad River, then winds its way up to Irasville Common and Village Square. Crossing Route 100 into the Mad River Green, the path continues to the Skatium, Winter Park and ends across from the Couples Athletic Field."¹

Warren Village has one existing sidewalk along the west side of Main Street. It begins at the Warren Store and ends just before the intersection of Fuller Hill Road. A sidewalk was proposed as part of the Warren Village Pedestrian Enhancement Plan on the west side of Main Street starting across from Fuller Hill Road to the bridge over Freeman Brook just south of the Warren Store.

¹ "Mad River Valley Map & Guide to trailheads, parks, rec. paths & day trips & Long Trailhead Hikes, 2nd Edition"; Map Adventures Topographic Maps & Guides.



Figure 9: Waitsfield Village Existing Pedestrian Network



Bicycle Travel

Bicycle travel is accommodated with paved shoulders along VT 100 and VT 17. Shoulder widths in the study area are shown in Figure 10. In general, the shoulder widths south of North Fayston Road are at least three feet wide, which is acceptable for shared use by experienced cyclists. North of Fayston Road, the shoulder widths vary from two feet to less than one foot. This width, in combination with limited sight distance caused by numerous curves through that area, creates less than desirable conditions for cyclists. The shoulder widths also decrease at spot locations along VT 100 and VT 17 due to guiderails and bridges.

Experienced cyclists are confident traveling on shoulders and have the skills necessary to handle locations where shoulders widths decrease. On-road travel is not necessarily recommended for less experienced cyclists or children.

Mad River Greenway

There are numerous hiking and walking trailheads in the Mad River Valley that provide a valuable recreational resource. The trailheads, which are discussed more fully as part of the inventory of recreational resources in other parts of the byway management plan, are destinations in and of themselves.

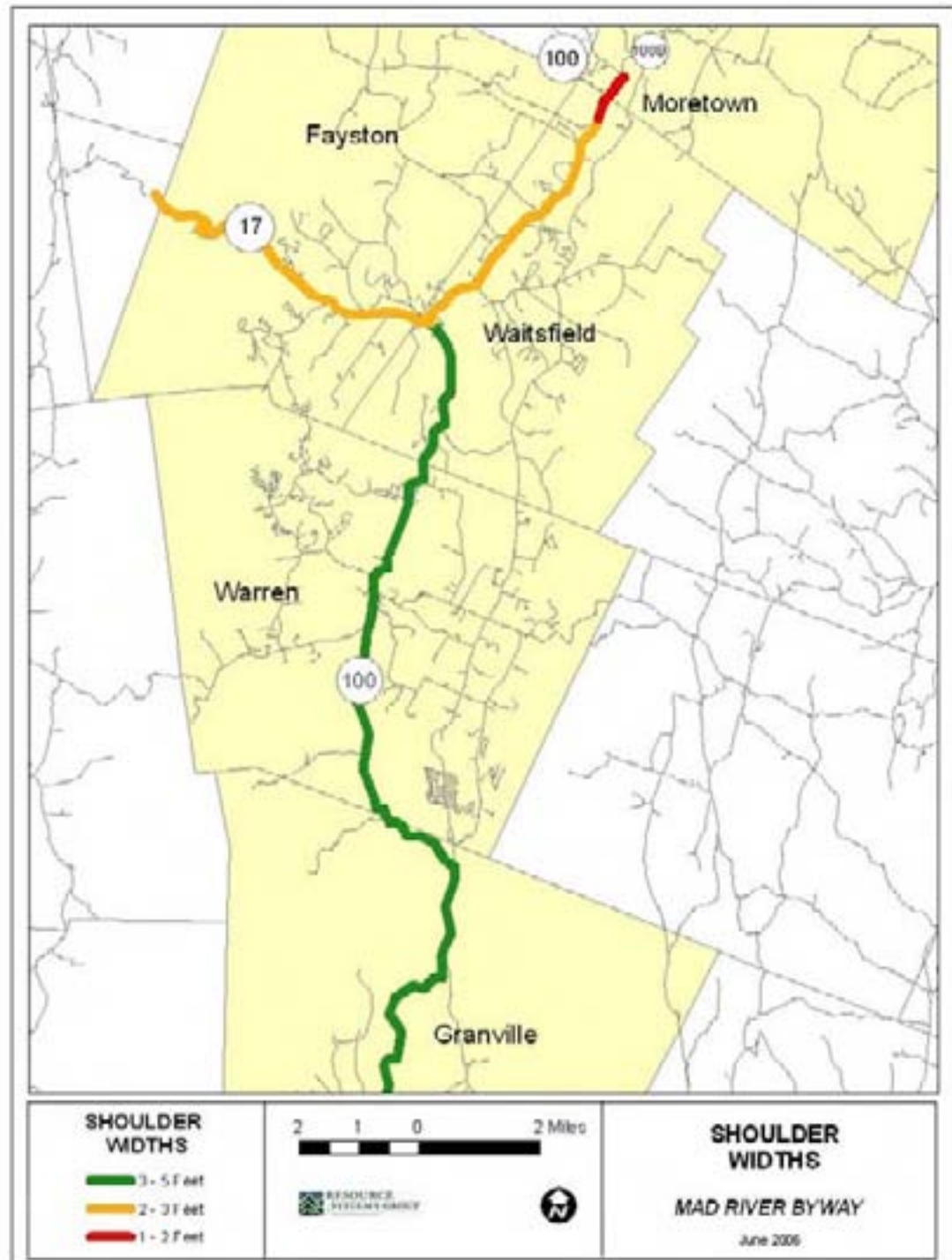
The Mad River Greenway is unique among trailheads in the Valley. While it is also a recreational destination, when complete the Greenway will provide an alternate route to VT 100 for travel on foot and bicycle during warm weather, and by ski and snow mobile during winter months. The Mad River Greenway (sometimes referred to as the Mad River Path) is being developed by the Mad River Recreation Path Association and will eventually link Moretown, Waitsfield, Irasville, and Warren Villages.

Currently, the Mad River Greenway is located on the east side of the river and extends from Tremblay Road to a dead end approximately $\frac{3}{4}$ of a mile north of Meadow Road. Parking areas are located on Tremblay Road, approximately 0.60 miles east of VT 100, and on Meadow Road. The path is up to seven feet wide with a mowed grass surface. Because it is located on private land, the Greenway's existence and continued expansion depend on the willingness of private land owners to allow access.

In order to achieve the vision of the Path Association to create a continuous greenway between all four villages, easements and rights-of-way should be acquired. The acquisitions could occur through direct purchase from willing land owners (possibly with funds available through the byway program), donations by land owners, or through negotiations that occur in the development review process. Acquisition of this type, which involves multiple private land owners and various approaches, is a long-term endeavor. The completion of the Mad River Greenway should be included in the byway management plan as both a transportation and recreational resource.



Figure 10: Shoulder Widths



TRANSIT

Transit service is provided by the Mad Bus, which is operated by the Green Mountain Transportation Agency (GMTA). With the exception of the Snow Cap route, the fare is free. The service runs from December to April to meet the needs of ski area visitors and employees. During the rest of the year, the Mad Bus is only operational for special events (Warren 4th of July Parade). GMTA offers limited transit services by reservation only between Montpelier, Moretown, and Waitsfield. The service connects all of the major destinations within the Mad River Valley but does not provide a regular connection to points beyond. GMTA also serves the Waitsfield Senior Center.

Table 3: Mad Bus Characteristics

Route Name	Extent of Route	Fare	Days	Hours
Snow Cap Commuter	Montpelier H.S., Sugurbush, Mad River Glen	\$2.00	Weekends and Holidays	2 Roundtrips/Day at 8:00 AM and 3:00 PM
Mount Ellen	Sugurbush North to Sugurbush South	Free	Mon-Sun	8:00 AM - 5:00 PM
Mad River Glen	Sugurbush, Mad River Glen, VT 100-VT 17	Free	Weekends and Holidays	7:30 AM - 9:30 AM, 2:20 PM - 5:45 PM
Harwood Free Rider	Harwood Union HS to Sugurbush	Free	Tue - Thu	1 trip at 2:50 PM
Saturday Evening Service	Irasville to Sugurbush	Free	Sat	6:00 PM - 2:00 AM
Valley Floor	Waitsfield Vil, Warren Vil, Sugurbush	Free	Mon-Sun	7:00 AM - 6:00 PM
Access Road	Southface to Lincoln Peak	Free	Mon-Sun	8:00 AM-11:30 AM, and 5:30 PM



Figure 11: Mad Bus Routes¹

TRAFFIC VOLUMES

Figure 12 shows the year 2000 Average Annual Daily Traffic (AADT) volumes in terms of vehicles per day along VT 100, VT 100B, and VT 17. Although the data are over five years old, they provide a comparison of relative traffic volumes throughout the corridor. The highest traffic volumes occur on VT 100 between VT 17 in Irasville and Bridge Street in Waitsfield Village. This section of the corridor is a major destination point because of the services available in Irasville and the Village. The AADT decreases south of VT 17, which helps distribute traffic from VT 100 to final destinations. Traffic volumes decrease significantly south of Warren. This pattern indicates that the VT 100-VT

¹ Source: <http://www.gmtaride.org/main.php/BusInformation/MadBusSchedules>



100B intersection is the primary gateway into the Mad River Valley, while VT 100 to the south and VT 17 are much secondary entrances and exits.

Figure 13 shows how traffic volumes vary by month at the continuous traffic counter located just north of VT 17 on VT 100. The figure shows the average weekday and weekend AADT. Weekend traffic, at least at this location, is higher than weekday traffic, underscoring the effect of tourism on the transportation system.

There are two peaks in AADT during the year. The first peak is during the winter ski season from January to March. During this period weekend traffic volumes are significantly higher than weekday traffic volumes. A second peak, although not as pronounced as the winter peak, occurs from mid-summer to mid-fall. Causes for this peak include summer recreational activities and fall foliage tourism. During this seasonal peak the weekend traffic volumes are higher than the weekday traffic volumes, but the difference is not as large as during the winter peak.



Figure 12. 2000 AADT

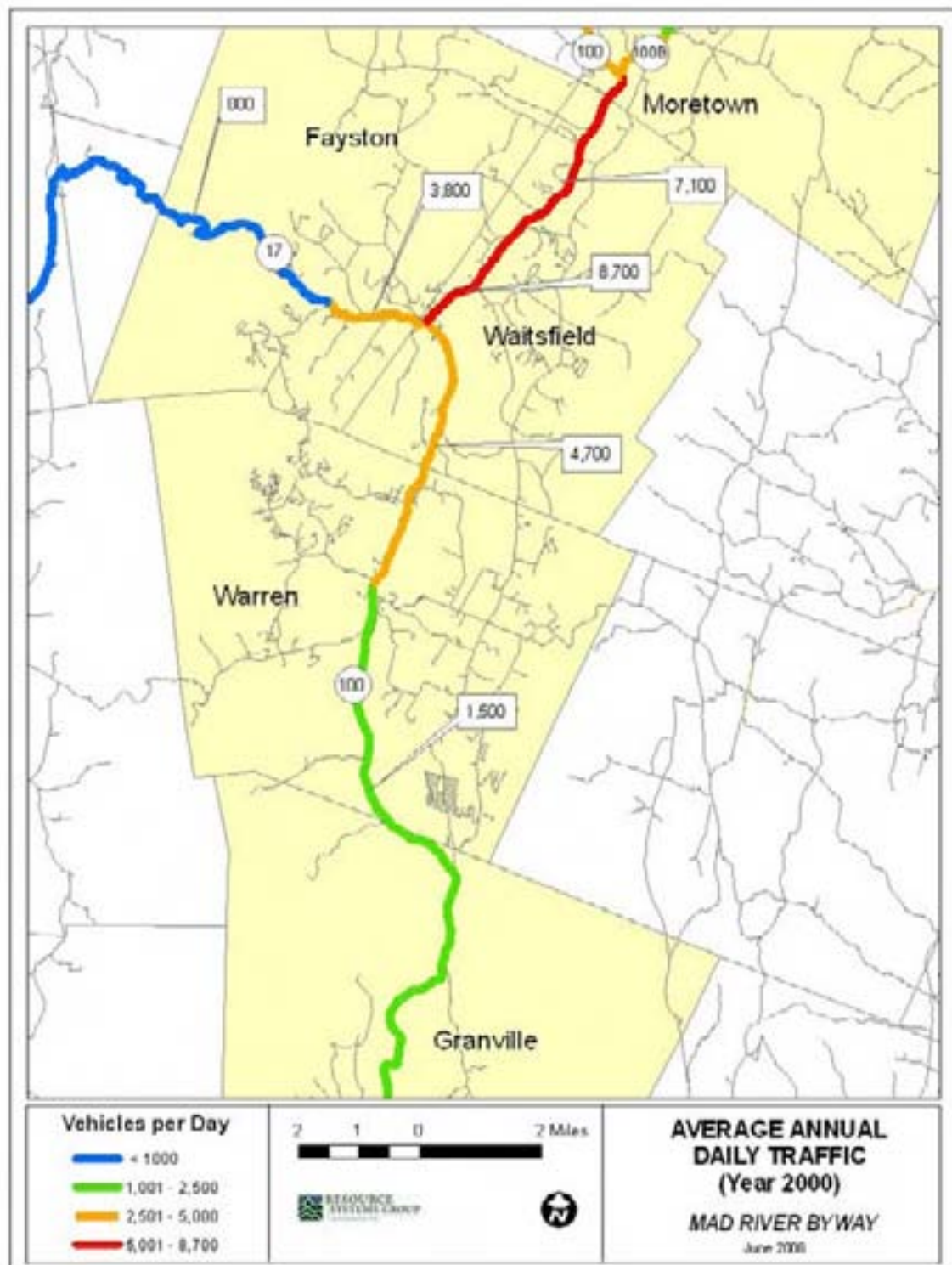
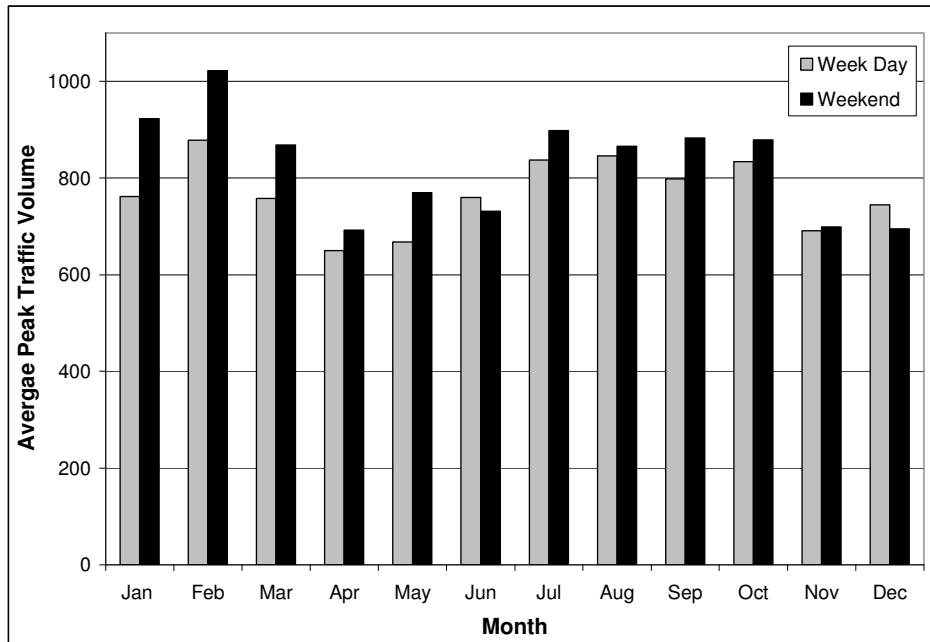


Figure 13. 2004 AADT Seasonal Variations

CONGESTION

Level of Service (LOS) is the standard measure used to quantify congestion of intersections and road segments as perceived by the driver. The grades A, B, C, D, E and F are the six possible LOS ratings. An LOS A indicates that the facility is operating exceptionally well with free flow, while an LOS F indicates that demand exceeds capacity. There is almost universal agreement that levels of service A, B and C are acceptable and LOS F is not. Whether or not LOS D is acceptable depends on the location of the intersection or road segment in question.

On rural highway facilities where speeds are often higher and drivers expect a higher level of mobility, LOS D may not be acceptable. In urban areas and activity centers where drivers expect and are accustomed to greater delays, an LOS D is often widespread and considered acceptable. In some cases, LOS E may be acceptable in urban areas and activity centers.

The VTrans policy on level of service is:

- LOS C is desirable for rural facilities;
- LOS D is desirable for urban facilities; and
- LOS E or F may be permitted in an urban setting if the remedy, such as adding new lanes, would significantly impact the surrounding natural or built environment.

Level of service for both signalized and stop-controlled intersections is measured in terms of average delay per vehicle and has been calculated according to chapters 16 and 17 of the 2000 Highway Capacity Manual (HCM). The delay, referred to as control delay, includes the time required to slow



down when approaching an intersection, the time a vehicle is stopped, the time required for a line of vehicles (the queue) to move up to the intersection, and the time required to accelerate. Table 4 presents the relationship between LOS and control delay.

Table 4. Relationship between Level of Service and Delay for Intersections

LOS	Characteristics	Stop-Controlled Delay (Seconds)
A	Little or no delay	< 10
B	Short delays	> 10 and < 15
C	Average delays	>15 and < 25
D	Long delays	> 25 and < 35
E	Very Long delays	> 35 and < 50
F	Extreme delays	> 50

The Central Vermont Region Transportation Plan points out that “In general, there is congestion along the corridor at Route 17, at the shopping centers, and during ‘event’ days (i.e. ski area closing time, summer concerts, and Columbus Day weekend)”¹. This congestion is most acute at the VT 100 intersections with VT 17 and Bridge Street. Previous studies have documented the congestion issues at these intersections as follows:

- VT 100-VT 17: The VT 17 approach is projected to operate at LOS F (average delay of 235 seconds/vehicle) in 2005 during the PM peak hour. In 2025, the VT 17 approach is projected to operate at LOS F during the AM (89 seconds/vehicle) and PM (2141 seconds/vehicle) peak hours².
- VT 100-Bridge Street Intersection: In 2005, the Bridge Street approach to VT 100 operates at LOS E (average delay of 48 seconds per vehicle) in the AM peak hour and LOS F (average delay of 110 seconds per vehicle) in the PM peak hour. By 2010, vehicle queues on Bridge Street are projected to reach the Covered Bridge.³

It is not unusual for the side street approaches to an arterial highway to operate at LOS E or F during peak traffic periods. However, these two intersections foreshadow an issue that will become more common, even on lower volume roads and some driveways. As traffic volumes increase on VT 100 it will become more difficult to enter the traffic stream from side roads and driveways. In village centers, this problem can be partially addressed by reducing speeds, which provides more time and opportunity to enter the traffic stream. Other options, such as redirecting traffic to select

¹ 2003 Central Vermont Region Transportation Plan, page 199.

² “Fayston-Waitsfield RSRGC-RS0200(7) VT Route 17 over Mill Brook (Bridge No. 38) VT Route 17/VT Route 100 Intersection Initial Scoping Report”; pages 16-18, Dufresne-Henry for the State of Vermont Agency of Transportation; December 1999

³ “Waitsfield Village Parking and Pedestrian Circulation Study”, Prepared by RSG; Draft Report, September 9, 2005.



intersections where traffic control devices such as traffic signals or roundabouts create a break in the traffic stream, should also be considered.

Table 5 shows the level of service for segments of VT 100 and VT 17 in the study area in 2000 and 2020. The analysis was taken from the 2003 Central Vermont Region Transportation Plan. The table shows the target LOS. Along rural sections, the target is LOS D and in village areas, the target is LOS E. The targets reflect differing expectations. Drivers expect a higher level of mobility on rural road sections and are more willing to accept congestion in village or other built-up areas. The targets are satisfied in 2000 and 2020 for all of the road segments in the study area. This finding suggests that there is not a need to consider additional through lanes on VT 100 or VT 17 in the foreseeable future.

Table 5: Road Segment Level of Service¹

Roadway Segment		Unacceptable LOS per CVRPC Policy	Average Annual Daily Traffic		Volume to Capacity Ratio		Level of Service	
			2000	2020	2000	2020	2000	2020
VT 100	South of Warren Village	D	1,100	1,628	0.06	0.09	A	B
	Between Warren and Waitsfield	D	4,700	6,956	0.19	0.29	C	C
	Irasville (north of Route 17)	E	6,500	9,620	0.27	0.37	C	D
	Waitsfield Vill (at Bridge Street)	E	8,700	12,876	0.36	0.49	D	D
	Between Waitsfield & S Duxbury	D	7,100	10,508	0.29	0.40	C	D
VT 17	West of Irasville	E	3,800	5,624	0.16	0.23	C	C
	West of German Flats Road	D	1,000	1,480	0.06	0.08	A	B
	West of Mad River Glen	D	790	1,169	0.04	0.06	A	A

SAFETY ANALYSIS

VTrans collects and analyzes crash records from around the state. Crash reports are filed by the Vermont State Police when a fatality or incapacitating injury is involved, and/or property damage equals or exceeds \$1,000.

In order to put these numbers into perspective, the number of crashes is divided by the number of vehicles passing through each road section. The result is a crash rate which can be compared to all other road sections and intersections on the state system. A road segment or intersection is identified as a High Crash Location (HCL) when the actual crash rate (crashes per million vehicles) exceeds a critical crash rate and there are more than five crashes in five years (or more than 1.0 crashes per year). The critical rate varies by functional class and is calculated using the most recent statewide averages that are developed by VTrans.

¹ Central Vermont Region 2003 Transportation Plan, Table 23. Page 64.



The only HCL identified by VTrans in the study area¹ is a section of VT100 in Warren north of the Kingsbury Bridge. Three of the five crashes at this location occurred during snowy or foggy conditions. Two of the five crashes were due to a “failure to keep within the proper lane.” Two of the five crashes were due to “driving too fast for conditions.” One of the crashes was due to a “failure to yield right of way.”

Sight distance and other potential safety issues are identified under the field assessment of recreation/cultural resource access points in the next section of the report.

FIELD ASSESSMENT OF RECREATION/CULTURAL RESOURCE ACCESS POINTS

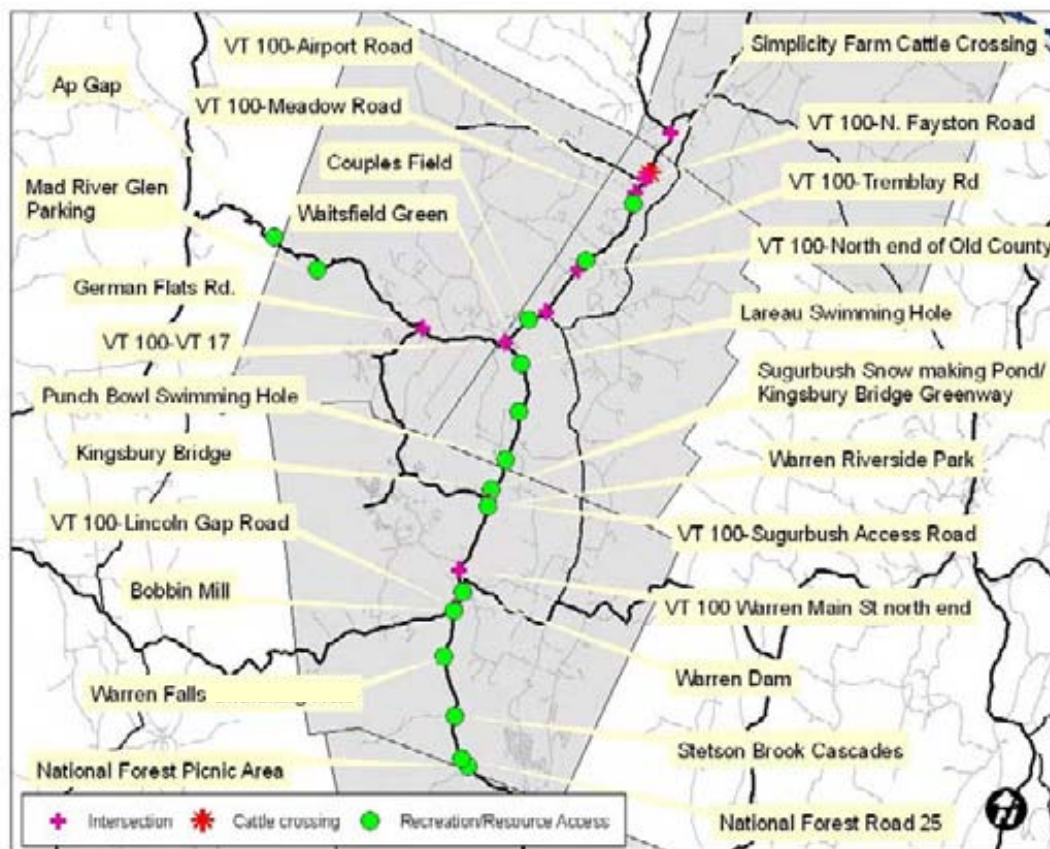
VT 100 and VT 17 provide access to the numerous recreational, cultural, and natural resources shown in Figure 14. Locations of interest were identified by the steering committee at the start of the planning process. A site visit was made to each location to document the type of use, available sight distance, posted speed, lighting, natural and built environmental constraints, pedestrian and bicycle accommodations, and opportunities for enhancements or solutions to address any identified deficiencies.

VT 100 and VT 17 each provide direct access to many of these resources. Several intersections with local roads were also evaluated because they connect VT 100 or VT 17 to other resources not directly abutting either highway.

¹ Based on a review of the VTrans “Statewide Intersections – Ranking by Actual/Critical Ratio, Years 1998-2002” and “Statewide Sections Route Log Order/2 - Statewide, Years 1998-2002”



Figure 14: Study Sites



Site (North to South)	Use	Sight Distance	Design	Posted Speed	Lighting	Constraints	Ped/Bike	Other Notes and Recommendations
VT 100-VT 100B	Intersection	No obstacles looking north, 490 ft available to the south.	One lane approaches, VT 100 southbound is wide enough for defacto right-turn lane. Stop sign on VT 100 southbound	40 mph	None	Barn on southwest corner	2-3 foot shoulders on VT 100 and VT 100B	<ul style="list-style-type: none"> Behavior of youthful drivers creates safety concerns VTrans has reconstruction design Potential location of an informational kiosk. Congestion worse in AM Lighting is recommended at this intersection because it is difficult to see at night.
Simplicity Farm Cattle Crossing	Cattle crossing	Limited to the south – flashing beacons provided	Located on horizontal curve just north of narrow bridge over Sheppard's Brook	50 mph	None	Adjacent to brook Change in grades not conducive for underpass tunnel	Poor sight distance and narrow shoulders at bridge	<ul style="list-style-type: none"> Bridge is 28 ft curb-to-curb with 10 ft lanes 2-4 ft shoulder Owner has requested cow tunnel from VTrans but there are many constraints
VT 100-N. Fayston Road	Intersection. Provides access to Burnt Rock trailhead	Adequate both directions	One lane approaches, stop controlled on N. Fayston Rd.	50 mph on VT 100, 35 mph on N. Fayston Road	None	None	No Issues	<ul style="list-style-type: none"> No changes recommended.
VT 100-Airport Road	Intersection. Provides access to Mad River Industrial Park	Adequate both directions	One lane approaches, stop controlled on Airport Rd	50 mph on VT 100, 35 mph on N. Fayston Road	None	None	No Issues	<ul style="list-style-type: none"> No changes recommended.
VT 100-Meadow Road	Intersection. Access to Mad River Greenway Parking	Marginal. May be restricted when leaves are on trees	"Y" configuration. Meadow Road approaches VT 100 at sharp angles. Also directly adjacent Small Dog Electronics driveway	50 mph	None	Steep grade and limited space from Mad River to VT 100	Once in parking area, no issues. Access from VT 100 difficult due to grade, intersection configuration and limited sight distance	<ul style="list-style-type: none"> Parking area serves 3 trailhead-heads Swimming hole and VAST access Not many viable reconstruction alternatives available Consider keeping as "local" access and guide visitors to other parking areas
VT 100-North end of Old	Intersection	Limited to north by	Old County Rd has skewed	50 MPH	None	Ravine between Old County Road	No issues	<ul style="list-style-type: none"> Reducing grade on VT 100 would help increase sight distance but would be



Site (North to South)	Use	Sight Distance	Design	Posted Speed	Lighting	Constraints	Ped/Bike	Other Notes and Recommendations
County Road		hump in road	approach to VT 100			and VT 100		expensive
VT 100-Tremblay Rd	Intersection	Adequate both directions	One lane approaches, stop controlled on Tremblay Road	50 mph	None	None	No issues	<ul style="list-style-type: none"> Access to Pines Rest Area Picnic Area, Mad River Greenway Trailhead Head 0.6 miles east VAST trailhead head Good location for tourists because its close to VT 100, visible, and has safe access. Connect Mad River Greenway to this parking lot
Couples Field	Driveway on height of land between Irasville and Waitsfield Village	225 ft to north 300 ft to south	Gated driveway	30 mph	None	Horizontal and vertical alignment of roadway and steep grade down to field	Adequate shoulders and existing sidewalk on east side. Village Path trailhead across the street	<ul style="list-style-type: none"> Consider pedestrian crossing at this location to connect Village Path to Couple's Field Look for alternative access points to Couples Field
Waitsfield Green	Shopping center and green space used for Farmers Market	OK	Multiple access points and rear parking through Mad River Green shopping area	30 mph	Yes	None	Village Path passes through the Green. VT 100 Transportation Path will further improve pedestrian and bike access	<ul style="list-style-type: none"> No changes recommended.
VT 100-VT 17	Intersection	Limited	"Y" configuration with island creates multiple conflict points	30 – 40 mph	Yes	Grade, buildings, vertical and horizontal alignment	No sidewalks. Shoulders adequate for cyclists.	<ul style="list-style-type: none"> Provide minor access management improvements in the short-term Long Term – Reconstruct the intersection to eliminate the "Y" configuration
Lareau Swimming Hole	Swimming hole and parking area	Adequate both directions	Driveway	40 mph	No	None	Bike lane pavement marking	<ul style="list-style-type: none"> No changes recommended.
Munn Field	Future town recreation area	Adequate along all of parcel's road frontage	No official access currently exists	50 mph	No	None	Bike lane pavement marking	<ul style="list-style-type: none"> Consider providing access on Kingsbury Road at south end of parcel rather than on VT 100
Punch Bowl Swimming Hole	Swimming Hole	Adequate both directions	Informal driveway adjacent to residential drive. Not well defined. Parking area provided	50 mph	No	None	Bike lane pavement marking	<ul style="list-style-type: none"> Consider consolidating swimming hole and residential driveways and aligning with Yestermorrow Driveway



Site (North to South)	Use	Sight Distance	Design	Posted Speed	Lighting	Constraints	Ped/Bike	Other Notes and Recommendations
Kingsbury Bridge	Truss highway bridge	Adequate	21.5 feet curb-to-curb	50 mph	No	Limited travel lane width. Bridge is considered important to the landscape but is not identified as historic	No shoulders for on-road bicycle travel	<ul style="list-style-type: none"> This section of VT 100 is identified by VTrans as a High Crash Location Disagreement exists on the design for the bridge's eventual replacement Any design will incorporate connection across the river for the Kingsbury Bridge Greenway as it continues to the Warren Riverside Park
Sugurbush Snow making Pond/Kingsbury Bridge Greenway	Trailhead access. Future site of affordable housing	Adequate both directions	Driveway	50 mph	No	Floodplain and town conserved land on-site	Bike lane pavement marking	<ul style="list-style-type: none"> No changes recommended.
VT 100-Sugurbush Access Road	Intersection. Access to ski area	Adequate	One lane approaches, stop controlled on Access Rd. Stop bar located 43 ft back from edge of travel lane	50 mph	No	None	No issues.	<ul style="list-style-type: none"> Used to have traffic signal when bridge on VT 100 was being repaired Driveway to commercial building on NW corner is too close to the intersection. Relocate to the west.
Warren Riverside Park	Swimming hole and summer recreation	Adequate both directions	Driveway	50	No	No	No issues.	<ul style="list-style-type: none"> The entrance is not well marked. Install an entrance sign.
VT 100 Warren Main St north end	Intersection	Adequate both directions	"Y" Configuration with island	50	No	No	No issues.	<ul style="list-style-type: none"> Town prefers that the current "Y" configuration remain in place. The bridge and curves further along Main Street slow traffic as it enters the Village
Warren Dam	Interesting feature	NA	NA	25	No	Close to edge of river	No direct pedestrian connection.	<ul style="list-style-type: none"> Consider providing overlook and on-street parking Connect to center of Village with sidewalk
VT 100-Lincoln Gap Road	Intersection, and Access to Lincoln Gap and Long Trailhead	Adequate both directions	"Y" Configuration – with painted lines and wide throat	50 mph	No	Grade down Lincoln Gap Road	No issues.	<ul style="list-style-type: none"> Closed to through traffic in Winter The configuration is too wide and may be confusing. Consider making the entrance more narrow.
Bobbin Mill	Intersection to Trailhead head, stream and river access and mill site	Adequate both directions	Intersection of Town Highway that serves Bobbin Mill	50 mph	No	Near brook	No issues.	<ul style="list-style-type: none"> Plenty of room for parking by the Bobbin Mill.
Warren Falls	view the falls	Marginal depending	Open pull off area. Spill over	50 mph	No	Limited space between road	No issues.	<ul style="list-style-type: none"> Add warning signs in advance of the site from both directions



Site (North to South)	Use	Sight Distance	Design	Posted Speed	Lighting	Constraints	Ped/Bike	Other Notes and Recommendations
		on where vehicle exits	parking located to the north on east side of road			and river to reorganize parking without removing nice trees and other vegetation that contribute to the attraction of the site		
Stetson Brook Cascades	National Forest Road and trailhead	Adequate both directions	Town road winds past a couple of houses and converts to National Forest road	50 mph on VT 100	No	Homes along town road may be impacted if traffic increases	No issues.	<ul style="list-style-type: none"> Consider plowing road to parking area for winter recreational use Provide signs at VT 100 intersection
National Forest Picnic Area (6.8 miles north of Granville and just north of NF Road 25)	Picnic Area and pull off	Marginal		50 mph	None	None	No issues	<ul style="list-style-type: none"> No recommended changes.
National Forest Road 25 @ Warren Town Line	Winter and summer recreation	Adequate	Forest Road intersection	50 mph	None	None	No issues.	<ul style="list-style-type: none"> Uses VTrans turnaround at Warren/Granville town line for parking area Install Parking Sign to formalize parking for this location.
VT 17-German Flats	Intersection. Provides access to Fayston Elementary and Sugurbush	Limited from German Flats Road approach looking east due to vegetation	"T" configuration with single lanes on all three approaches. German Flats approach is stop controlled. VT 100 is on a sharp curve .	40 mph	None	Brook directly adjacent to VT 100. German Flats Road approach passes over large culvert.	No issues.	<ul style="list-style-type: none"> Keep brush clear on VT 100 approaches to provide maximum sight distance.
Mad River Glen Parking	Parking for ski area	Acceptable both directions	Wide open continuous curb cut along entire length of VT 17	40 mph	None	Peak parking demand utilizes entire parking area	No issues	<ul style="list-style-type: none"> Short-term <ul style="list-style-type: none"> Warning sign – Watch for Turning Vehicles Regulatory sign – Parking prohibited in shoulder Long-term <ul style="list-style-type: none"> Satellite parking at location to be determined served by shuttle



Site (North to South)	Use	Sight Distance	Design	Posted Speed	Lighting	Constraints	Ped/Bike	Other Notes and Recommendations
								<ul style="list-style-type: none"> Reconfiguration of VT 100 parking lot with proper access design, parking space layout and circulation.
Appalachian Gap Overlook	Pull-off and scenic view at the top of the gap. Long Trailhead.	Limited looking from the pull-off to the east	Continuous curb cut on horizontal alignment and at crest of long, steep hill.	40 mph	None	Paved pull-off area bound by ledge to the north, VT 17 to the south, and drop-off and guiderail to the west.	No Issues	<ul style="list-style-type: none"> Reconfigure entire area to provide designated parking spaces for cars and motorcycles. Provide boundary between VT 17 and parking area and formalize the entrance. Provide green space and basic amenities for people to enjoy the view.



SUMMARY OF ISSUES AND GENERAL RECOMMENDATIONS

The municipalities and citizen groups in the Mad River Valley, Central Vermont Regional Planning Commission, and the Vermont Agency of Transportation, have been involved in a continuous planning process that has identified roadway, bicycle, and pedestrian facility improvements along VT 100 and VT 17. These previous recommendations and projects provide the foundation for the proposed byway's transportation management plan.

The roadway's alignment consists of frequent horizontal and vertical curves. These features sometime appear to reduce sight distance or cause a brief reduction in shoulder width. However, they also make the road more interesting and may be considered as valued characteristics. With the exception of the high crash location near the Kingsbury Bridge, crash records suggest that the roadway is safe. Therefore, there is no need to make significant modifications to the roadway's alignment or cross-section. If and when sections are reconstructed and redesigned, the existing alignment should be preserved, minor shoulder widening included where feasible to improve conditions for on-road bicycle travel, and as noted in the Vermont State Design Standards, advisory speed limits posted where appropriate.

VT 100 and VT 17 have sufficient capacity to accommodate projected traffic volumes. Therefore there is no need to consider widening these roads to four lanes in the foreseeable future.

Existing congestion is limited to the minor road approaches to VT 100 at VT 17 and Bridge Street. The congestion occurs in the afternoon and morning peak hours and is most intense during weekend event days (i.e. ski area closing time, summer concerts, and Columbus Day weekend). These two intersections foreshadow an issue that will become more common, even on lower volume roads and some driveways. As traffic volumes increase on VT 100 it will become more difficult to enter the traffic stream from side roads and driveways. Redirecting traffic to select intersections where traffic control devices such as traffic signals or roundabouts create a break in the traffic stream should be considered.

There is one high crash location in the proposed byway near the Kingsbury Bridge on VT 100. The fact that there is only one HCL in the corridor indicates that VT 100 and VT 17 provide safe transport for vehicles relative to other similar highway facilities in the state. However, Valley residents have expressed concern with the speed of vehicles traveling through Waitsfield Village, Irasville, and Warren Village. Although both VT 100 and VT 17 provide a high level of safety for vehicles, pedestrians and cyclists do not always feel safe traveling along these roadways, even within village centers.

The speed of vehicles traveling on arterial highways through village centers is a common issue in Vermont. In the proposed extension of the Mad River Byway, the issue is of particular concern in Waitsfield Village and Irasville, which are located directly on VT 100. The Waitsfield Village Parking and Pedestrian Circulation Study (2006) includes a traffic calming plan for the Village that consists of gateways, bulbouts and curb extensions, on-street parking, and raised pedestrian crossings. These



recommendations are necessary to preserve and enhance the quality of life, aesthetics, and economic vitality of the Village and should be included in the management plan for the byway.

Sidewalks currently exist in Waitsfield Village, Irasville, and Warren Village. The VT 100 Transportation Path will enhance pedestrian facilities in Waitsfield and Irasville. Recent plans have identified additional pedestrian improvements for Waitsfield Village and expansion of the sidewalk in Warren.

Shoulder widths decreased near guiderails and at bridges but were otherwise adequate for on-road bicycle travel along VT 17 and VT 100. Shoulder widths should be increased along VT 100 between North Fayston Road and VT 100B, if and when that section is reconstructed. As VTrans replaces the guiderails, box beam should be used which will leave more space for the shoulders.

Paved shoulders are adequate for experienced cyclists. The Mad River Greenway when complete will provide an alternate route to VT 100 for all types of cyclists and pedestrians. Because it is located on private land, the Greenway's existence and continued expansion depends on the willingness of private land owners to allow access. In order to achieve the vision of the Path Association to create a continuous greenway between all four villages, easements and rights-of-way should be acquired.

Sight distances were adequate at most of the access points to the recreational and cultural locations evaluated along VT 100 and VT 17. If a car was entering or exiting the entrance, there is enough sight distance. However, if there were no vehicles using the driveways, many of the entrances to the points of interest are difficult to see. Consistent signs at the entrance are recommended.

MAD RIVER BYWAY EXTENSION TRANSPORTATION MANAGEMENT PLAN

The following table combines the projects identified from previous plans and studies with the ideas generated as part of the analyses and field work conducted for this report into an overall transportation management plan for the extension of the Mad River Byway.

Table 6: Mad River Byway Extension Transportation Management Plan

Location	Recommendation
Mad River Greenway: Moretown-Waitsfield- Irasville-Warren	Extend the Greenway to connect all four Mad River Valley villages. In the short-term, continue to work with land owners to secure permission to expand the Greenway. Begin to acquire easements and rights-of-way to secure permanent access to existing segments of the Greenway and to continue its expansion to all four village centers.
Reconstruction and design policy for VT 100 and VT 17.	When reconstructing road segments, maintain the existing alignments to preserve the roadways' character and include minor shoulder widening where feasible to improve conditions for on-road bicycle travel.
Recreation and Cultural Resource Locations	Provide consistent signs with a Mad River Byway theme to mark entrances from VT 100 and VT 17.



Location	Recommendation
Transit	When demand warrants, increase the frequency of the Mad Bus/Mad River Glen Shuttle and extend the service to Moretown Village, and expand service for the entire year.
VT 100-VT 100B Intersection	Implement the VTrans reconstruction design, include appropriate lighting, and construct an information and visitor kiosk on the northwest corner in conjunction with a park and ride lot.
VT 100 from VT 100B to North Fayston Road	Reconstruct the road to include wider shoulders on the existing alignment. Include improvements to the Simplicity Farm Cattle Crossing as part of the roadway project.
Meadow Road Mad River Greenway Parking and Access	Maintain this location for local access and guide visitors to other Greenway parking areas. When the Greenway is expanded, this parking area should not be shown on maps that tourists use.
Tremblay Road Parking Area	Provide a clear connection between the parking lot near VT 100 and the Mad River Greenway with directional signs. In the long-term, the Mad River Greenway should connect directly to this lot.
Waitsfield Village and Irasville	Complete the VT 100 Transportation Path
Waitsfield Village Pedestrian and Streetscape Enhancements	Implement the recommendations of the 2006 Parking and Pedestrian Circulation Plan: <ul style="list-style-type: none"> ▪ Sidewalk, green-strip, access management, and on-street parking on the west side of VT 100, intersection and mid-block pedestrian crossings over VT 100 ▪ Street trees and pedestrian scale lighting. ▪ Parks at General Wait House and Valley Players Theatre ▪ Place utilities underground
VT 100-Bridge Street Intersection	Short term – Bulbouts, enhanced pedestrian crossings, and definition for the Farr Lane entrance. Long Term – Traffic signal.
Couple's Field Entrance	<ul style="list-style-type: none"> ▪ Provide crosswalk to connect the Village Path to Couples Field. ▪ Investigate different vehicular access points
VT 100 between VT 17 and Couple's Field	Provide left-turn lanes at the entrances to the Mad River Green and Village Square, subject to satisfying traffic engineering guidelines.
VT 100-VT 17 Intersection	Short Term – Provide minor access management improvements at the northwest corner. Long Term – Reconstruct the intersection to eliminate the “Y” configuration
Munn Field	Provide access to Kingsbury Road, when the park is developed, rather than on VT 100.
Punch Bowl Swimming Hole	Consolidate swimming hole and residential driveway and align with Yestermorrow driveway.
VT 100-Sugurbush Access Road	Short-term: Provide traffic control officer during peak periods. Long-term: Replace the bridge on VT 100 just south of the intersection and include a rail design that does not obstruct sight distance. Reduce the vertical curve on VT 100 north of the intersection to increase sight distance.
Kingsbury Bridge	The bridge needs to be rehabilitated. The existing truss bridge could be replaced with a modern structure or rehabbed. Either way, the new bridge should accommodate cyclists traveling along VT 100 and include an underpass for the Kingsbury Bridge Greenway.
Warren Village Pedestrian Improvements	Implement the recommendations of the Warren Village Pedestrian Enhancement Plan. <ul style="list-style-type: none"> ▪ Extend the existing sidewalk on the west side of Main Street from Fuller Hill Road to the bridge over Freeman Brook ▪ Provide textured crosswalks at several locations, speed tables at the Village entrances,



Location	Recommendation
	and lighting and benches.
Warren Dam	Include landscaping, benches, and other amenities to create a formalized public space and overlook. Include a sidewalk to Fuller Hill Road (southern end of sidewalk recommended in Warren Village Pedestrian Plan)
VT 100-Lincoln Gap Road	Reconstruct the Lincoln Gap Road approach to applicable standards. It is too wide and lacks definition.
Warren Falls	Add warning signs on VT 100 in advance of the pull-off from both directions
Stetson Brook Cascades	Plow road to parking area for winter use
National Forest Road 25 at Warren/Granville town line	Install sign to formalize parking in pull-off area
VT 17-German Flats Road	Keep brush cleared along VT 17 to maximize sight distance
Mad River Glen Ski Area parking lot along VT 17	<p>Short Term</p> <ul style="list-style-type: none"> Warning sign – Watch for Turning Vehicles Regulatory sign – Parking prohibited in shoulder <p>Long Term</p> <ul style="list-style-type: none"> Satellite parking (location to be determined) served by shuttle Reconfiguration of VT 17 parking lot with proper access design, parking space layout and circulation.
Appalachian Gap Pull-off	Reconfigure entire area to provide designated parking spaces for cars and motorcycles. Provide boundary between VT 17 and parking area and formalize the entrance. Provide green space and basic amenities for people to enjoy the view.



ROUTE 100

VERMONT BYWAY CORRIDOR MANAGEMENT PLAN

A Vermont Byway is a highway or other public road that may be associated with transportation and landscape resources that have special scenic, natural and cultural qualities, and that has been formally designated as a Byway by the Vermont Transportation Board.

WE NEED YOUR HELP

FEBRUARY 09, 2006

7:00 PM - 9:00 PM

**@ YESTERMORROW
Design Build School**

*a PUBLIC MEETING to discuss the byway proposal,
present the resource inventory obtain public input and
support for the project.*

For more information, please contact:

Steve Gladczuk at the Central Vermont Regional Planning Commission
(802) 229-0389 or gladczuk@cvregion.com

Devon Pierce

(802) 496-7173 or mrvpd@madriver.com

David Raphael at LandWorks

(802) 388-3011 or info@landworksvt.com

Directions to Yestermorrow Design/Build School

From the North: Travel south on Route 100 from Waitsfield Village. Yestermorrow is approximately 2 miles south of the intersection of Route 100 and 17. Look for the Bundy sign on the left. Yestermorrow will be the next building on the left. Enter the driveway and continue up the hill to the parking area. The main entrance is the first door at the rear of the building.

7:00PM INTRODUCTIONS & PROJECT BACKGROUND

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7:45PM SMALL GROUP DISCUSSIONS

Participants will convene into smaller groups to discuss issues, needs, options for projects & programming, and a theme for the byway.

8:30PM RECONVENE & DISCUSS NEXT STEPS

Groups will reconvene and present the conclusions obtained during the discussion.

8:45PM NEXT STEPS & FINAL REMARKS

David Raphael will conclude the meeting by discussing the next steps in the planning process leading towards a successful nomination.

VERMONT ROUTE 100 BYWAY DESIGNATION PLAN

PUBLIC MEETING – THURSDAY, FEBRUARY 9, 2006

THANK YOU FOR ATTENDING THIS IMPORTANT EVENT!
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Route 100

VERMONT BYWAY

Corridor Management Plan

Proposal for Route 100

Route 100 follows the Mad River through one of Vermont's most unique and well-known regions, the Mad River Valley. With its classic agricultural traditions and rugged natural features, this mountain valley landscape is quintessential Vermont. Views of the Green Mountains towering over historic villages and barns, the roaring Mad River, distinctive architecture and covered bridges, bustling communities, and exceptional recreational opportunities characterize this route. This highway is the backbone of daily life for the byway communities, providing key access to residents and visitors who use the Valley's rich resources. In recognition of these values, the Central Vermont Regional Planning Commission, the Mad River Valley Planning District and the towns of Granville, Warren, Moretown, Fayston and Waitsfield are in the process of preparing a proposal to designate Route 100 a Vermont Byway, from the intersection with Route 100B south to the end of the Granville Gulf. The byway proposal seeks to establish a coordinated plan to enhance and promote the highway corridor and the many resources found within it. Designation as a Vermont Byway provides a formal way for Vermont communities to identify, conserve, and promote roads that have special qualities and resources, and then to make improvements along the corridor that benefit and sustain the byway and community interests. If designated, funds may be available through the National Scenic Byways Program to facilitate activities that support initiatives along



the corridor, including preservation, transportation, and economic development. This designation proposal recognizes the value and potential of Route 100 as a Vermont Byway.

Origins of Vermont Scenic Byways

In 1993 the state established the Vermont Byways Program using federal funds from the National Scenic Byways Program. The national program gives states "wide latitude" in defining their own ways of participation and Vermont has tailored its program to meet the objectives of its towns. The Vermont Byways Program aims to integrate growth management, economic development, and transportation investments while preserving the state's exceptional scenic, cultural, historic, and recreational qualities.

Vermont's Working Landscape: The Mad River Valley

As Route 100 travelers pass through the Valley, they are following an important north-south thoroughfare in this remote region. Yet more importantly, they are driving through main street communities, as Route 100 goes from one historic village to the next. The highway has existed in this manner since as early as 1807 when it was first surveyed as the Mad River Turnpike. The route provides unique opportunities for residents and visitors alike to access the variety of resources in the corridor from scenic views to swimming holes.



The visual experience along the byway is outstanding. The Mad River twists and turns, roaring from snowmelt in the spring and calming in the summer to form pools and gravel bars. Steep banks expose the river's impact on the underlying bedrock. Green in the summer and sparkling white in the winter, fields and meadows roll out from historic barns to the wild, glacially carved mountains that extend skyward on either side of the valley.

The story of the Valley's history is alive in the nationally recognized villages and districts made up of buildings and covered bridges dating back to the region's original settlement. While many old farms have been turned into inns or other businesses, the working landscape is still evident and vibrant in the area. The Mad River Green Farmer's Market is full of fresh local produce during the summer months, and farmers open their fields to recreational users during the winter. Sugaring shacks continue to boil maple syrup and grazing cows maintain meadows for open, spectacular views.

Route 100 also provides plentiful recreational opportunities. The region is well known for its two major ski areas. The Mad River, surrounding mountains, and open fields are also popular and easily accessible recreational resources. Swimming holes just off Route 100 are some of the best in the state. Public and private landowners have come together to create an extensive trail system that is used for hiking, snowshoeing, snowmobiling, or just a short walk along the river. These opportunities attract and relate people to the Route 100 corridor.

WE NEED YOUR HELP!

A Public Meeting will be held on **March 23, 2006 at 7:00 pm** at the **Waitsfield Elementary School** to discuss the byway proposal, what we've learned so far, to develop the next steps, and to formalize committee leadership.



Byway Objectives

The Vermont Byways Program establishes a process to identify, conserve, and promote roads that have special qualities. Local and regional objectives for the Route 100 Byway include:

☞ **Community Enhancement:** Maintaining the character of the valley by highlighting and addressing areas that need improvement, strengthening local economies, and educating residents and visitors alike. This objective should address the interests of landowners, businesses and local residents along the byway to create a locally defined corridor management plan.

☞ **Preservation:** Planning for the preservation and protection of important resources that are unique to the corridor through the creation of management strategies and plans. These plans should address the long-term goals for the corridor and provide continuity into the future.

“The Valley is a magical place and I now recognize what a lucky individual I am to have the privilege of growing up here. I would like to come back in 20 years and have my children see it the way I see it now ~ beautiful.”

~ Emily Norton, Warren, age 14; from *Kicking Stones Down a Dirt Road*, page 15

☞ **Recreation:** Providing improved access by encouraging a coordinated approach to the management of existing resources and development of new ones. This could include work on the Mad River Path system or a look at bicycle use in the Byway. Improving information distribution, signs, and parking should also be addressed.

☞ **Promotion:** Encouraging local and regional partnerships that serve residents and attract visitors and providing appropriate amenities for the public. This should include continued work on effective and integrated advertising that reaches beyond the Mad River Valley. Amenities could include redevelopment of the Top Gas station as a central place for tourist information and for local advertising opportunities.

☞ **Transportation:** Supporting mobility and safety improvements and facilitating various travel options. This should address traffic patterns through the byway villages including pedestrian/vehicle interactions, curb cuts, and village sidewalk systems.

Consideration and Benefits of Byway Designation

A number of considerations will guide development of the Byway and if designated, clear benefits will be seen in local communities and the region:

☞ The Byway program is designed to be a ***citizen based and community oriented*** initiative, addressing the needs and concerns of all local Byway residents and users.

☞ The Byway program promotes ***local and regional collaboration*** between public and private sectors to address common issues. Since the Route 100 Byway crosses through several towns, management and protection strategies will benefit from collaboration.

☞ The Byway program ***does not have regulatory power*** and designation does not require changes to property owner rights and zoning.

☞ The designation is ***flexible***. The Byway program encourages communities to choose a theme and purpose for their byway that is ***locally defined***.

☞ Conservation and management ***incentives are seen at the local level***, rather than by upper-level

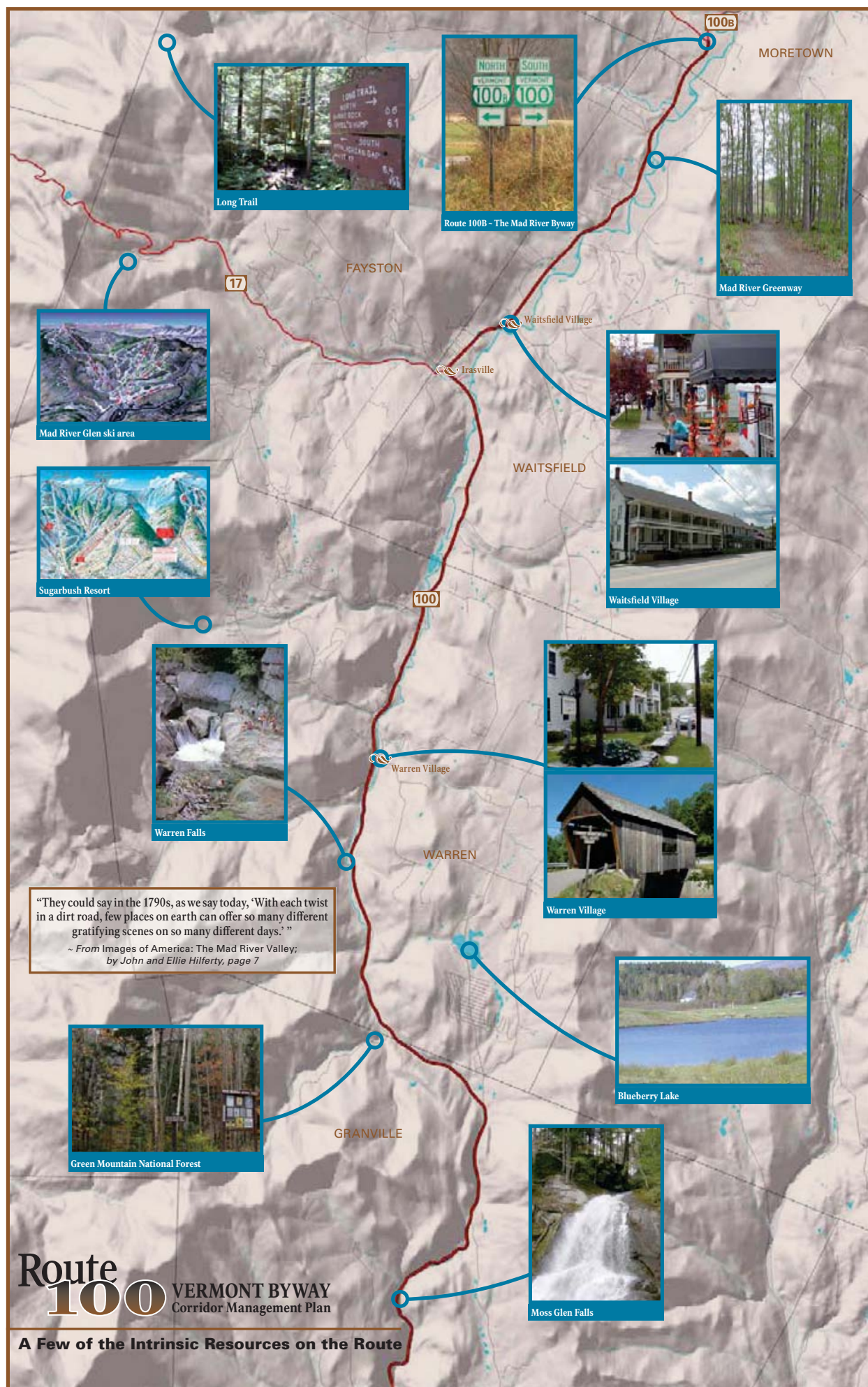
government. -If designated by the state, the Route 100 Byway would become eligible for ***federal funding*** from the National Scenic Byways Program. The funding could be used at the local level for a number of related improvements.

☞ Byway programs can ***set in motion regional initiatives*** and plans that are already under consideration for transportation, environmental planning and economic development.

☞ Byway designation serves as a ***promotional tool*** at the local, state and national level. The program automatically ties into state and federal advertising programs that are already in effect, with the federal program targeting national as well as international visitors.

What can you do?

The Vermont Byways Program emphasizes the value of local leadership and the importance of voluntary participation and grassroots work in support of the byway. The participation of local businesses and residents in the planning and promotion of the byway is essential. Local citizens, public officials, and property owners can attend byway planning meetings and work with local governments and organizations to support the byway project.



Route 100 VERMONT BYWAY Corridor Management Plan

A Few of the Intrinsic Resources on the Route

PUBLIC MEETING #2

Route 100

VERMONT BYWAY Corridor Management Plan

Thursday, March 23, 2006

7:00pm - 9:00pm

Waitsfield Elementary School

VT 100, Waitsfield Village

For more information:

<http://www.centralvtplanning.com/Publications.html>

Steve Gladczuk at the Central VT Regional Planning Commission

(802) 229-0389 or gladczuk@cvregion.com

David Raphael at LandWorks

(802) 388-3011 or info@landworksvt.com

We need your help!

Come **EARLY** starting at
6:30pm to view the displays
of inventory work and maps

EVENING AGENDA

7:00PM INTRODUCTIONS & PROJECT BACKGROUND

- Intro and Project Background
- Review Byways Program & Purpose
- Address how the Mad River Valley and Route 100 fits or can be planned as a Vermont Byway while maintaining local values and interests
- Present inventory summary and transportation assessment summary
- Present findings, results and recommendations from previous workshop and work to date.

7:45PM SMALL GROUP DISCUSSIONS

- Confirm, refine and further develop goals/objectives/strategies for the Corridor and the Corridor Management Plan

8:30PM RECONVENE & DISCUSS NEXT STEPS

- Groups will reconvene and present the conclusions obtained during the discussion.

8:45PM NEXT STEPS & FINAL REMARKS

- David Raphael will conclude the meeting by discussing the next steps in the planning process leading towards a successful nomination.

MARCH 23, 2006 PUBLIC MEETING

Thank you for attending this important event! Your ongoing input and support are critical for the successful designation and implementation of the Byway.

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1

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LOCAL AND REGIONAL OBJECTIVES

Five important local and regional objectives are proposed for the Route 100 Vermont Byway. These objectives provide the basis for identification, implementation, and management of the corridor's intrinsic resources and are intended to stimulate and promote economic, recreational, and environmental benefits for the corridor's communities and transportation systems. Most importantly, the objectives supply the foundation for the long-term goals and strategies, which will be developed as part of the Corridor Management Plan. The five objectives proposed for the Route 100 Vermont Byway, as well as some example goals and strategies for each, are found below. Please confirm, refine and further develop these proposed objectives, goals and strategies:

1. Multi-Modal Transportation

To provide reliable and harmonious multi-modal travel options.

Goal A. Improve pedestrian and bicycle mobility, activity, and safety within village centers and along the rural highway.

Strategy A.1 Develop and implement sidewalk upgrade and connectivity plans.

Strategy A.2 Widen and/or improve shoulders along Route 100 to accommodate bicycle and automobile traffic where needed.

Goal B. Implement and promote an integrated/linked transit system that allows commuter options as well as access into the region.

Strategy B.1 Evaluate current transit systems and identify key holes and missing links.

2. Recreation

To improve access to recreational resources and facilitate collaborative approaches to recreational development and management.

Goal A. Improve information, signs, and other wayfinding elements that direct visitors to various public and private recreation activities and destinations along the entire Byway.

Strategy A.1 Create a Valley-wide wayfinding and information program.

Strategy A.2 Create a sign plan for public swimming holes along the Byway.

Goal B. Promote and improve connectivity of the Mad River Pathway.

Strategy B.1 Encourage private landowners' co-operation.

LOCAL AND REGIONAL OBJECTIVES – DRAFT FOR REVIEW

3. Character

To preserve and enhance the rural character, village patterns and community values of the Mad River Valley.

Goal A. Support villages in efforts to create a sense of community.

Strategy A.1 Evaluate and develop plans for infill development to prevent sprawl.

Goal B. Create a stronger awareness among residents and visitors of points of historic significance along the Byway.

Strategy B.1 Work with the town historical societies to preserve the stories and memories of the people behind the historic landscape and buildings.

Goal C. Ensure that Byway development does not harm traditional land uses.

Strategy C.1 Develop incentive programs to keep historic farms operational.

4. Preservation

To plan for the preservation and interpretation of the unique scenic, historic, cultural, and natural resources found within the Byway Corridor.

Goal A. Open space, agricultural lands, and natural habitats should be protected and preserved.

Strategy A.1 Support local, regional, state, and private organizations' acquisition of land or conservation easements.

Goal B. Create stronger awareness among residents and visitors of the unique natural resources that are protected in the valley.

Strategy B.1 Encourage educational programs with local schools on protected public lands.

5. Promotion

To develop effective and integrated marketing and information strategies for Byway users and provide appropriate amenities for the traveling public.

Goal A. Provide central locations for information distribution and local advertisement opportunities.

Strategy A.1 Redevelop Top Gas property into tourist kiosk.

Goal B. Develop a solid framework and strategic program for the Byway that will market Route 100 as a destination itself in addition to the numerous resources available to the public.

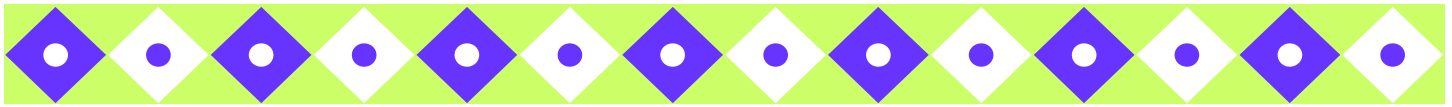
Strategy B.1 Create and maintain a Route 100 Byway website that links to points of recreational, cultural, and historic interest along the route.

VT 100 Meeting Sign In 3/23/06

NameContact Info

~~Jim Jones~~
 Harrison Snapp
 Mac Rood
 Don Wexler
 Russ Bennett
 Chuck Marten
 Sal Spinoso
 Susan Hemminger
 Jay Maciejowski
 Dan Currier
 Steve Glodzik
 Bill Heinzerling
 Polly McMurtry
 John Hildebrand
 Nancy Coombs
 Garry Kessler
 David Dixon
 Ted Tremper
 Brian Fleisher

TRS@BSECMANT.NET
 Waitsfield, VT.
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 Moretown, VT.
 Waitsfield
 Fayston
 Waitsfield
 Waitsfield
 Deer Isle, ME
 Dan Currier
 CVRPC
 Fayston, VT
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Public Hearing

**Towns of Warren, Waitsfield, Fayston, Moretown,
and, Granville Gulf Reservation**

Mad River Byway

**Route 100/17 Vermont Byway Extension
Corridor Management Plan**

Thursday, July 26

7PM

Warren Town Hall

7PM—Introduction and Project Background

Steve Gladczuk will introduce the project

**7:15 PM—Overview of the nomination package and Corridor
Management Plan**

**David Raphael will provide a summary of the Nomination
Package and Corridor Management Plan key elements and
goals and strategies**

8PM—Next Steps and Final Remarks

**David Raphael will conclude the hearing by reviewing next
steps, including approvals by six Select Boards and pres-
entation of plan to Scenery Preservation Council and
Vt Transportation Board**

**Questions—Steve Gladczuk—CVRPC 229-0389
Gladczuk@cvregion.com**

**Project Documents can be found at
<http://www.centralvtplanning.com/Publications.html>**

BYWAYS

PUBLIC HEARING

JULY 26, 2007

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EIN POST

VALLEY REPORTER

VK 2/9/06

Two public meetings to be held on scenic byway

By Katrina VanDyne
Route 100 and Route 100B are slated to become designated as scenic byways. There are two upcoming public meetings scheduled to discuss byway designation plans and details about the benefits of this program.

There will be a public meeting held on Thursday, February 9, at Sastemur's in Westfield to discuss Route 100 and a public meeting on Tuesday, February 14, at the Montown Town Hall to discuss Route 100B.

The scenic byway program is designed to help recognize, preserve and enhance selected roads throughout the state. A Vermont byway is defined as a highway or other public road associated with transportation and landscape resources that have special scenic, cultural and historical

qualities, and has been formally designated as a Byway or the Vermont Designated Tour Road. The goal is to bolster tourism, sustainable development and preservation with the needs of residents, according to Don Pierce of the Mad River Planning District.

The designation of 100B as a Vermont Byway will enable the town, organizations and regional entities to apply for federal funding for corridor management initiatives. Possible initiatives include scenic development efforts, village enhancements, tourism and recreation promotion, and multi-modal transportation improvements," according to Mac River Neighborhood Association website.

The meeting on Thursday, February 9, will include intro-

ductions, a prepared local economic group discussions and future steps in the planning process. Valley residents are encouraged to attend. The introduction of this meeting is to gain input from property owners along the corridor and people who use the road such as cyclists, commuters, tourists, etc. To get an agenda for Thursday's meeting and a list of discussion items, visit the Mad River Valley Planning District at 485-7173.

The Vermont Transportation Board will hold a hearing on the scenic byway program for Route 100B on Thursday and encourage residents to attend. As this is a hearing, that needs the strongest showing for more information on these days meeting, go to the Mad River Neighborhood Association website at www.VMNB.org.

Downsized and legally reviewed, Kiesel subdivision is back

By Lisa Loomis

A smaller and, according to engineer William McLean, simpler subdivision for Mark and Polly Kiesel's 150 acres on the Bowen Road in Westfield was presented to the town planning commission on February 7.

The Kiesel's came to the town

The second pending issue has to do with whether the 250 is needed now for a development with less than 800 feet of road. State statutes changed in 1975 when the Kiesel's first applied for an Act 250 permit, and now, under current statute, they should not need an Act 250 permit, but the

use. The town denied the request to amend and the court upheld the town's decision citing the Stowe Highlands Club case.

The fundamental tenets of that decision are that subdivisions can only be amended if very specific criteria are met. To be amended, there must be

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Public input sought on scenic roads in Waitsfield

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Waitsfield residents and those who use the town's scenic roads are invited to offer public comment on the future maintenance and designation of scenic roads in the town at a forum next week.

On Monday, March 20, at the first of a series of discussions on Waitsfield's designated scenic roads will be held at the town office. At this public forum, members of the select board and the planning commission's scenic roads committee will discuss how to act upon the principles of the town plan and how to balance scenic and recreational values with safety and serviceability.

The meeting takes place at 8 p.m.



Sherman Road in Waitsfield

Waitsfield

Scenic Byway meeting to be held

Members of the public are invited to attend a presentation and discussion of the Route 100 Byway initiative at the Waitsfield School at 7 p.m. on Thursday, March 23.

A Vermont Byway is defined as a highway or other public road associated with transportation and landscape resources that have special scenic, natural and cultural qualities, and has been formally designated as a Byway by the Vermont Transportation Board. The goal is to balance tourism, sustainable development and preservation with the needs of residents, according to Dee Pierce of the Mad River Planning District.

The Vermont Byways Program establishes a process to identify, conserve and promote roads that have special qualities, and often pass through scenic areas. The By-

way initiative is essentially a community-oriented program that seeks to address the needs and concerns of those that live within and use the Byway corridor.

At the first meeting to promote the Byway initiative several priorities were generated by those that attended: providing information to residents and visitors on the abundance of resources within the Route 100 corridor; extending the Mad River Path, and maintaining the quality of local villages for residents.

The Byway program relies on community members to choose a theme and purpose for the Byway that is locally defined. Additional participation by community members is necessary for the initiative to be successful and beneficial for the Valley.

The ideas, opinions and concerns of Valley residents are

welcome. Join others and participate in defining a successful Byway program for Route 100.

Byway brochures are available at The General Wait House and the Waitsfield and Warren Town Offices. For additional information contact Steve Gladczuk at Central Vermont Regional Planning Commission, 223-0389.



Proposed byway extension up for discussion

By Erin Post

A public hearing next Thursday is slated to give Valley residents a peek at a proposal for a state-recognized byway stretching from Montpelier to Granville.

The meeting takes place on July 26 at 7 p.m. at the Warren Town Hall.

An official from the Central Vermont Regional Planning Commission as well as a consultant working on the proposal are scheduled to be on-hand to present the "central management plan" and answer questions.

TOWN LINE

The project would extend the recently designated Mad River Byway — which encompasses Route 1003 in Moretown — south through Waitfield, Warren and Granville Gulf reservation. The byway has also been proposed to include Route 17 from its intersection with Route 100 to just short of the town line with Buell's Cove.

Pursuing a byway designation recognizes all towns through which the byway would pass to sign off on the proposal. The plan is then presented to the state's Scenic Preservation Council and the Vermont Transportation Board for review.

According to the introduction of the byway corridor management plan, the proposal would recognize Route 1003's scenic qualities and historic significance as well as help to "establish a coordinated plan to enhance and promote the highway corridor and the many resources and attractions found along it." If the byway is approved, funds may become available through the national scenic byways program "to facilitate planning activities" for initiatives along Route 100.

PRIORITIES

Some priorities identified by the byway steering committee include redeveloping the former Top Gas station as a tourist

information kiosk, improving safety and parking at trailheads, developing a coordinated sign plan for the byway and working with the state to make improvements to Granville Gulf reservation.

Improving the Route 100/Route 17 intersection was also identified as a main concern, as was upgrading the Mad River Path and Greenway.

The byway steering committee, Mad River Valley Planning District representatives and other local residents have helped to generate the proposal, according to the central management plan.

For more information, contact the Central Vermont Regional Planning Commission at 229-0889 or by emailing glades@regplan.com. Project documents can be viewed at www.centralplanning.com/publications.html.

Moretown voters to reconsider MLL agreement in late August

By Erin Post

Dates for a public meeting and second town vote on an agreement with Moretown Landfill Inc. (MLLI) have been set for next month.

"There's a lot of misinformation out there," he said, adding that the steel may be helpful if it addresses questions residents have posed at prior meetings and to men-

being targeted to host an additional cell for waste. Construction of a fourth cell may extend the life of the landfill by as many as 15 years, landfill officials have said.



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TOWN & REGIONAL PLAN EXCERPTSGranville Town Plan Excerpts**HOUSING***Goals (p. 9)*

- (3) To encourage the preservation of historic structures in ways that appropriately serve the need for housing.

TRANSPORTATION*Background (p. 10)*

Since Route 100 and Route 12A are maintained by the state, any increase in traffic will not place any burden on the Town.

Goals (p. 10)

- (1) To maintain a transportation system that is safe, efficient and complements the other goals and policies of this Plan.
- (2) To ensure that future development does not unnecessarily or unreasonably endanger the public investment in town and regional transportation systems or facilities, including highways, bikeways, trails, and rail.
- (3) To support local, regional and state-wide efforts to provide public transportation systems that meet the needs of all population segments.
- (4) To minimize transportation energy consumption.

Policies (p. 10-11)

- (1) Prior to a final decision to proceed with a major capital transportation project, policy makers should first analyze the project against reasonable alternatives. In examining the alternatives, investigation should focus on the environmental, energy, social and investment costs and the extent to which each meets the goals and policies of this Plan.
- (2) It is in the public interest to maintain the Town's current highways, bridges, and related facilities as it is necessary to ensure the current level of service. It is the Town's policy not to accept any new Town roads.
- (4) Given the interest in and benefits from biking, hiking, snowmobiling, cross-country skiing, and similar outdoor recreational activities, the Town shall, as an alternative to complete discontinuance of a highway, give full consideration to preserving Class 4 roads for recreational use, or downgrading their status to a legal trail thus retaining the public's interest in them.

(5) It is in the public interest, that major construction or reconstruction shall consider paths and trails for use by the pedestrian or other non-conventional modes.

(6) An integral scenic element of the rural countryside is the network of backroads comprising the Town's highway system. These byways are both visually and economically important to the Town. If improvements are needed to accommodate increased traffic, it is important to consider the relationship of the road to the surrounding features of the landscape.

(7) At this time, the Town of Granville is not inclined to participate in the Vermont Scenic Byways program.

HISTORIC, ARCHEOLOGICAL, AND SCENIC RESOURCES

Background (p. 11)

Most development resulting from change has adapted very well to our historical and cultural landscapes, our lifestyles, and community values. Generally, the old and the new have been complementary. It is the intent of this Plan to keep this sense of community. In spite of the successes, change brings potential for degradation and needless destruction of our past.

Policies (p. 12)

(1) It is the intent of this Plan to preserve outstanding scenic areas and to maintain the relationship between countryside and village, and to discourage developments which have an undue adverse impact on locally recognized scenic resources. As a means of maintaining or enhancing the scenic values existing in Granville, the following design considerations shall be considered in creating plans for new development:

place powerlines, towers, roads, drives, and similar structures in a manner as to reduce any visual impact.

(2) Land development should be planned so as to minimize the unnecessary loss of historic or archeological resources determined to be of local, state, or federal significance. In areas determined archaeologically sensitive, landowners are encouraged prior to construction to contact the State Archeologist for assistance.

(5) Public improvements, such as road rebuilding, and utility construction, should be developed to avoid undue impacts on significant historic or archeological resources or scenic areas.

ECONOMY

Locations of Economic Activity (p. 21)

Historically, Granville has not served as an economic hub for commercial and industrial activity for area towns and Addison County in general. Granville is a satellite community to Rochester and to a greater extent to

the Warren/Waitsfield area. Residents obtain banking, medical, professional and related services in Randolph, Waitsfield, Rochester, Montpelier/Barre, and Middlebury. Routes 100 and 12A are the primary access roads to Granville. Given its location within the backbone of the Green Mountains, east/west mobility within the town and to neighboring towns is limited.

LAND USE

Goals (p. 25)

- (1) Future land use needs to maintain an identity for Granville as a distinct community.
- (4) Future land use needs to retain clear evidence of the community's history while giving consideration for future needs.
- (5) Future land use needs to respect the community's identity and share qualities of scale and form with existing development.
- (7) Future land use needs to allow easy access to the natural environment and protect it from destruction.
- (11) Enhance outdoor recreation opportunities and participation by supporting open access to a variety of well-maintained motorized and non-motorized trail systems on public lands.
- (12) Inventory all “ancient roads” not currently shown on the Official Town Highway Map but which still exist as a public right-of-way, and make decisions as to their future use as roads or trails.

Fayston Town Plan Excerpts

Chapter 2. Goals and Objectives

Historic & Archaeological Resources (p. 4)

Goal 3.1 The preservation and celebration of Fayston’s unique history

Objective 1: To increase understanding and awareness of Fayston’s historic settlement patterns and development.

Objective 2: To protect and preserve, where possible, historic buildings, structures and archaeological sites significant to Fayston’s history.

Physical Features & Natural Resources (p. 5)

Goal 4.2 The maintenance, preservation and enhancement of Fayston’s natural features and environmental quality for the benefit of future generations.

Land Use Goals and Objectives (p. 10)

Goal 6.1 Guide land development in a manner which preserves important community resources, while encouraging a range of land uses in the appropriate locations.

Goal 6.2 Maintain a reasonable balance between community-imposed limitations on land use and the rights of individual land owners.

Transportation Goals and Objectives (p. 12)

Goal 8.1 The maintenance and provision of a safe, efficient and convenient highway network.

Goal 8.2 The improvement and expansion of alternative, non-automobile transportation modes.

Objective 2: To maintain and improve the function of Route 17 and Route 100 as regional arterial highways to the Mad River Valley.

Implementation Strategies:

- a. Work with other Valley towns and the Vermont Agency of Transportation to ensure that the function of Route 100 as a primary arterial is not diminished and that corridor issues are addressed in a cooperative manner.
- b. To improve access to the Mad River Valley, encourage the improvement of the Route 17 and Route 100 intersection and the Hyde Away corner on Route 17.

Objective 3: To provide alternatives to the heavy reliance on individual automobiles.

Implementation Strategies:

- a. Develop, in conjunction with the Mad River Path Association, other Valley towns and the Vermont transportation Path Program, a network of pedestrian and bicycle paths linking Valley towns.
- d. Encourage VAOT to provide bicycle lanes along Route 17.
- g. Encourage the expansion of the VAST trail network throughout Town.

Objective 4: To continue planning for Fayston's transportation needs in a comprehensive manner and coordinate these efforts with neighboring towns.

Recreation Goals and Objectives (p. 16)

Goal 10.1 The provision of adequate recreation facilities for Fayston's citizens.

Objective 1: Encourage Valley wide development and sharing of recreational facilities

Goal 10.2 The expansion of recreational facilities in accordance with the Town's land use policy.

Chapter 3. History and Historic Resources*3.4 Preserving Fayston's History (p. 22)*

... preserving the Town's early years requires attention to planning for preservation of historic structures and archaeological sites.

3.4.2 Incentives and Regulations for Historic Preservation (p. 23)

The Town may wish to consider creating incentives for the preservation of historic and archaeological resources. Some options may include participation in a Valley barn restoration fund, providing guidelines for protecting features such as stonewalls and cellar holes when land is subdivided or developed. Continued Town support for events like the Bicentennial also helps educate residents on Fayston's rich history.

Chapter 4. Natural Environment*4.1 Introduction (p. 24)*

Perhaps more than any other single factor, the Town's rugged natural environment distinguishes Fayston from its neighbors in the Mad River Valley and presents distinct issues for land use and natural resource planning.

Chapter 6. Land Use*6.1 Introduction (p. 48)*

The scenic quality of the Town is highly valued by residents and visitors and is considered one of the Town's main assets.

The Town Survey 2000 indicates that residents feel strongly about preserving the high quality of the rural landscape, and are concerned about environmental protection and the impact of future development on the Town.

6.2.2 Scenic Quality (p. 50)

The scenic quality of the area can't be underestimated. Fayston is composed of steep-sided hills and valleys, upland plateaus, fields, forest, mountain streams and small winding roads. The beauty of the surrounding landscape contributes greatly to the local economy and the quality of life in Town. It is the primary reason for attracting high value vacation home development to the area.

Chapter 8. Transportation*8.1 Introduction (p. 65)*

Ensuring residents can safely get from place to place on Fayston's sometimes challenging roads is a key function of Town government.

8.2.2 Existing Conditions: Collector Highways (p. 66)

Curb Cuts on Collector Roads

Some specific existing access points of concern include poorly-located commercial access points and roadways on Route 17...

Chapter 10. Recreation*10.1 Introduction (p. 92)*

The abundance of opportunities for recreation is one of the primary reasons that many people decide to live in Fayston, and it contributes to the high quality of life and sense of community prized by residents.

10.5 Planning and Policy Issues for Recreation in Fayston (p. 96)

Setting policy for the use and expansion of the Town's rich recreational resources should be a consideration for the Town. ... The town should work in partnership with the ski areas, as well as other public and non-profit organizations, on recreation initiatives that serve the best interest of the residents and property owners of Fayston.

Waitsfield Town Plan Excerpts**3. Natural Resources***Overview (p. 16)*

The integration of natural and cultural features create a distinct sense of "place" that is unique to Waitsfield.

Natural Resources Goals (p. 29)

- The responsible stewardship and sustainable use of Waitsfield's natural resources in a manner that protects and enhances the Town's environmental well-being for the benefit of future generations.
- The preservation of natural features that contribute to Waitsfield's ecological health and biological diversity.

Natural Resources Policies (p. 29)

2) The protection of identified natural resources shall be accomplished through measures and programs that support, where appropriate, the sustainable use of those resources, including management of productive forests, agricultural uses of productive soils, commercial and non-commercial recreational use of land and water, and the generation of renewable energy in appropriate locations.

4) Support the efforts of local, regional and statewide conservation organizations to protect open space in Waitsfield through voluntary programs (e.g., purchase or donation of development rights).

Priorities for open space protection include:

- a. productive agricultural land and working farms;

- d. contiguous wildlife habitat and travel corridors;
- e. trail corridors, river accesses and areas for dispersed recreation (e.g., hunting);
- f. riparian lands and floodplain;
- g. identified scenic viewsheds; and
- f. the above features where they serve to characterize and/or support the Valley's historic landscape, including land in the Mad River Valley Rural Agricultural District and land adjacent to designated growth centers which reinforce the contrast between compact village and open countryside.

4. Historic & Cultural Resources

Cultural Resources- Traditional Settlement Pattern (p. 33)

Waitsfield's traditional settlement pattern contributes significantly to the town's scenic character. The town is blessed with one of the most extraordinary scenic landscapes in Vermont. Encompassing a pleasant blend of rolling meadows, wooded hills, a meandering river, and imposing historic structures, the town's landscape is a source of pride to residents and an important attraction to visitors. Historic settlements, open farm fields, forested hillsides and ridgelines, and tree-lined roads are all important scenic resources. The preservation of the town's historic and scenic character is important for a variety of reasons: to promote tourism, to preserve the agricultural land base, to enhance recreational opportunities, and to protect important natural and cultural landscape features.

Cultural Resources- Scenic Resources (p. 36)

As mentioned above, Waitsfield's traditional settlement patterns and associated rural landscape is the community's greatest aesthetic resource. This landscape consists of several key features, however, that have been identified as distinct scenic resources... These features include:

- open farmland and meadows, which often serve as the foreground for expansive views;
- forested knolls, steep mountain-sides and ridgelines which provide the unbroken background for most distant views, most significantly land above an elevation of 1500' and lower hillsides and forested knobs that rise steeply to the east of the Mad River (between the River and the Waitsfield Common/East Warren plateau) and are highly visible from Route 100;
- the historic context of development, including compact villages surrounded by open land and the relationship of clustered farm buildings (of mixed scale and massing) surrounded by farmland;

- scenic roads, especially those of a scale and character that discourage high speed travel while offering a pleasant walking and recreational environment;
- the Mad River and adjacent riparian land and floodplain;
- individual buildings which, because of their scale, character or historic significance, such as a large barn, serve as a visual and cultural focal point in the landscape;...

Cultural Resource Protection (p. 36)

The 2002 Community Survey reconfirmed findings from earlier surveys—that there is a great deal of local support for preserving the town’s rural character, including its traditional settlement patterns, and historic, scenic and recreational resources.

Historic & Cultural Resource Goal (p. 37)

- To identify, protect and preserve Waitsfield’s cultural landscape and resources, including its traditional settlement pattern, historic build environment, and scenic features.

Historic & Cultural Resource Policies (p. 37)

9) Those scenic features within the rights-of-way of designated scenic roads, including but not necessarily limited to road width, surfacing materials, bordering trees, walls and fences shall be protected and maintained in accordance with an adopted municipal scenic road maintenance program.

14) Incentive and assistance programs and other nonregulatory means of cultural and scenic resource protection shall be encouraged where feasible, in accordance with adopted resource protection plans.

7. Local Economy

Economic Development Policies (p. 67)

3) c. Waitsfield Village will continue to serve as a commercial, cultural and civic center of the town, with an emphasis on businesses that are compatible with the Village’s scale and residential character.

4) The town will encourage the maintenance and expansion of the local tourism industry by supporting efforts to protect the town’s historic and natural resources, and expanding recreational and cultural opportunities for local residents and visitors.

8. Transportation

Transportation Goal (p. 80)

- To provide a safe, efficient and convenient transportation network for all Waitsfield residents, visitors and businesses.

Transportation Policies (p. 80)

- 1) The town shall pursue a program of transportation planning which integrates road and infrastructure improvements with land use, housing, economic development and resource protection goals and policies, and which is coordinated with surrounding towns in the region.
- 6) The safety and efficiency of state highways and town roads shall be maintained through, in part, strict access management provisions administered through local land use and development regulations.
- 10) Scenic roads shall be maintained to protect, to the extent possible, those features located within the right-of-way which contribute to the road's scenic features (e.g., canopy trees, stone walls); ...
- 11) Development within scenic road corridors shall be designed so as to protect the identified scenic features within those corridors (e.g., open fields, ridgelines and hilltops, historic structures or districts). Protection measures shall be implemented through the town's land use and development regulations.
- 13) Irasville and Waitsfield Village should be served with an interconnected network of sidewalks and other pedestrian and bicycle paths, including incorporation of identified road and sidewalk connections into development and subdivision plans. New development shall provide such sidewalks and paths to be connected to existing or planned facilities.
- 14) The creation of an integrated walking path network – through acquisition, easements or use of landowner agreements – linking Waitsfield Village and Irasville with the Valley schools, and the village centers of Warren to the south and Moretown to the north, and other community centers and resources, including the Lareau Swimhole, Skatium and Harwood Union High School, is part of the transportation and recreation plan for Waitsfield and the Valley. To this end, the efforts of the Mad River Path Association are strongly endorsed.
- 15) The continued operation of a transit system linking Waitsfield Village and Irasville with the Valley's ski areas and regional population and employment centers should be maintained and expanded. To this end, shelters and designated stops, sidewalks and park & ride facilities within village centers should be created and/or expanded.
- 16) Safe, well marked bicycle lanes should be installed and maintained along Route 100, Route 17 and, to the extent practical, along paved class 2 town roads.
- 18) Explore with appropriate landowners, the creation of a municipal parking lot in Waitsfield Village behind the Federated Church/Joslin Library, off Parsonage Lane and/or other appropriate locations.
- 19) Continue to coordinate transportation planning with other Valley municipalities and the Central Vermont Regional Planning Commission.

9. Community Facilities & Services

Community Services- Recreation (p. 93)

The Mad River Valley offers a rich variety of recreation opportunities to year-round residents, seasonal home owners and visitors. ...

- The Mad River Greenway – an extremely popular five mile grass path along the Mad River north of the Tremblay Road. The Greenway is maintained through a partnership between the Town, the Mad River Path Association, and landowners. Similar partnerships maintain a Village path network. This network could form the core of what could eventually be a single pathway linking Irasville with Warren Village to the south and the Fayston Elementary School to the west, and linking Waitsfield Village with Moretown Village and Harwood High School to the north.

Community Facilities & Services Policies (p. 100)

9) Opportunities for the town to acquire land for conservation, recreation and community facilities are encouraged and should be explored. Priority should be given to parcels which provide multiple values to the community.

11) The efforts of the Mad River Path Association to create a network of walking and bicycling paths in the Mad River Valley, including extending the Mad River Greenway to link Waitsfield Village with Moretown Village to the north, and with Warren Village to the south, are strongly supported.

To this end, the town will assist the Path Association by:

- a. holding easements on segments of the path right-of-way;
- b. incorporating path easements into required open space and pedestrian connections as part of local development review processes; and
- c. incorporating path segments into management plans for town-owned land.

13) The town will continue to encourage the efforts of VAST to provide an integrated network of winter recreation trails in a manner that does not adversely impact neighboring homeowners and the natural environment, and will allow VAST trail use of Class 4 roads on a case by case basis to avoid conflict with other users of the road and neighboring residential properties.

23) The Town will continue to work with and provide support, to the extent practical, to community arts and cultural organizations.

12. Land Use

Current Land Use (p. 121)

A dominant characteristic of Waitsfield is the extensive forest cover, especially in mountainous areas and on steep slopes rising from the valley

floor. The Town's physical character, however, is defined by the contrasting patchwork of that forest with large areas of farmland, especially in the vicinity of Waitsfield Common and the Valley floor, and an attractive built environment.

Rural & Conservation Districts (p. 122)

Agriculture-Residential District

- **Rural Character.** The landscape within the Agricultural-Residential District is a critical element of Waitsfield's rural character and special charm. The term rural character may be ambiguous, although in 1988 an attempt was made to identify the landscape features which define the Mad River Valley's rural character. The Mad River Valley Rural Resource Protection Plan loosely defines rural character as the historic working landscape formed by open farmland, barns, farmhouses and other historic structures, the scenic backdrop formed by forested hillsides and mountains, tree shaded roads and a meandering river.

Maintaining the Town's rural character has long been a planning goal of the community, and continues to be emphasized within this district. Methods to maintain the rural landscape include continued support and protection of the "working" features of the working landscape – agriculture and forestry – and balancing that with the careful development of new housing. ...

- **Agriculture & Forestry.** ... Maintaining a land base for farming and forestry not only helps to maintain Waitsfield's rural character and working landscape, it contributes to the local economic base in a sustainable manner.
- **Residential Hamlets & Density.** ... Incentives to encourage higher density residential development and new residential "hamlets"

Villages & Growth Centers (p. 130)

Waitsfield Village

- **Village Character.** As was explained in Chapter 4, Waitsfield Village is listed on the National Register of Historic Places as a Historic District. This designation recognizes the historic mix of architectural styles present in the village, as well as its representation of the classic New England development pattern characterized by a compact development center, dominant public buildings and the contrast between the village and surrounding countryside. While the village's historic character is largely intact, much of the contemporary development at the northern end deviates from the traditional styles found elsewhere in the village. A Historic Village Overlay District was adopted in 2002 to ensure that future development is compatible with the Village's historic character, and to discourage the demolition of historic structures.

Discouraging single-story architecture also should help maintain the Village's traditional scale, and careful consideration should be given to the impact of signage and lighting to avoid detracting from the pleasant streetscape found in the Village. Opportunities for enhancing the streetscapes should also be pursued, including planting trees to reinforce a canopy along Route 100 and Bridge Street and to ensure the continued presence of shade trees in front and side yards.

Finally, an important element of the Village's historic character is the sharp contrast between the compact village and the surrounding countryside. The wooded hillside west of the Village, and farm fields to the east, are subject to conservation easements and will remain undeveloped. The Mad River and the exposed ledges and winding character of Route 100 to the south also define strong boundaries and a clear southern gateway to the Village. The area to the north of the village, however, including agricultural land bordering Route 100, is critical to maintaining the sharp distinction between the village and the surrounding countryside. Although the Village Residential District begins adjacent to the General Wait House, the northern boundary of the Waitsfield Elementary School property and the row of pine trees north of the Waitsfield Telecom property create a strong northern gateway.

Land Use Goals (p. 137)

- The preservation of Waitsfield's historic settlement pattern, defined by compact villages surrounded by rural countryside.
- The regulation of land development in a manner which protects important cultural and natural resources while encouraging a range of land uses in appropriate locations.

Land Use Policies (p. 137)

- 1) Maintain the Town's historic settlement pattern of compact growth centers surrounded by rural countryside in accordance with the land use plan and associated land use districts described in this Chapter and depicted on Map 10.
- 3) Maintain the **Forest Reserve District** for the purpose of protecting significant forest resources and headwater streams and to limit development in areas with steep slopes, shallow soils, wildlife habitat, fragile features, scenic resources and poor access to Town roads, facilities and services. ...
- 4) Maintain the **Agricultural-Residential District** for the purpose of supporting the continued operation and expansion of agricultural operations, forest management, the preservation of rural resources and natural features, and to accommodate low density residential development

while encouraging moderate or high density clustered residential development in appropriate locations. ...

5) Land within the **Commercial-Lodging District** should continue to support agriculture, the preservation of rural resources and natural features, and accommodate moderate or high density clustered residential development and appropriate non-residential uses, in appropriate locations. ...

6) Ensure that development within Waitsfield Village is compatible with the historic scale and pattern of development, and with historic architectural styles, and discourage the demolition or inappropriate alteration of historic structures.

7) Maintain the **Waitsfield Village Residential District** for the purpose of allowing high density mixed use development, especially housing, in a classic village setting in a manner compatible with the historic character of the Village.

Warren Town Plan Excerpts

Chapter 2. Community History

Town History Goal (p. 2-9)

Goal 2.A Promote a greater understanding and appreciation of Warren's history and cultural heritage.

Town History Objectives (p. 2-9)

Objective 2.1 To celebrate and maintain Warren's history and cultural heritage.

Chapter 3. Sense of Place

Natural Resources (p. 3-2)

Warren's natural resources are among the town's most valued assets. ... Warren's natural resources are fragile and especially susceptible to degradation due to land use and development activities. Many serve important ecological functions, such as water filtration, wildlife habitat and stormwater retention.

Cultural Features (p. 3-11)

Cultural resources help us understand and celebrate our community heritage. They can be archaeological sites; historic sites, structures or settlement patterns; and larger cultural landscapes that reflect the character of a time, place or economy. Warren's cultural resources offer a link to the past, help define the town's present character, and provide a context for future growth and development.

Archaeological Sites (p. 3-12)

The town should use the development review process to require the protection of these remnants of the town's early history.

Historic Sites and Structures (p. 3-12)

Maintaining the historic character of Warren Village was one of the highest priorities expressed by participants at the 1997/1998 Town Plan meetings. Residents raised concerns regarding perceived threats to the residential character of the village, the need to maintain its historic architectural heritage and problems related to traffic, parking and pedestrian circulation.

Scenic Landscape (p. 3-13)

The road network is perhaps the most significant vantage point from which Warren's visual beauty may be enjoyed. Particularly where major arterials pass through open agricultural areas, the potential for spectacular long and intermediate views can be realized

Sense of Place Goals (p. 3-17)

Goal 3.A The maintenance, careful stewardship, preservation and enhancement of Warren's natural resources and environmental quality for the benefit of future generations.

Goal 3.B The preservation of the town's rural character, cultural heritage and historic working landscape.

Sense of Place Objectives (p. 3-17)

Objective 3.1. To protect Warren's fragile features, open space and natural resources.

Objective 3.2. To prevent degradation of water resources and improve water quality.

Objective 3.3. To protect and enhance Warren's wildlife populations.

Objective 3.5. To maintain an adequate land base to support present and future forestry and agricultural activities.

Implementation Strategies

b) Encourage the permanent protection of farmland and important natural resource areas, through conservation easements or comparable deed restrictions, during the subdivision and/or development review process.

e) Support the efforts of private conservation organizations to protect farmland, forest land and other open space in Warren through landowner education and voluntary conservation mechanisms.

i) Support a viable commercial recreation industry to encourage seasonal diversification and multiple uses of farm and forest land.

Objective 3.7. To protect and enhance Warren's scenic landscape and rural character.

Implementation Strategies

g) Continue protecting those features within the road right-of-way that contribute to the scenic character of individual roads through the town's road ordinance. Coordinate the protection of those features with road maintenance and improvement projects.

Objective 3.9. To promote traditional access to undeveloped lands for public recreation.

Implementation Strategies

a) Promote continued access to private lands for hunting, fishing and other forms of outdoor recreation, with due consideration given to landowner concerns such as liability, vandalism, safety and intrusion.

b) Support the efforts of the Mad River Path Association, Vermont Association of Snow Travelers, Catamount Trail Association, U.S. Forest Service and other parties to create and maintain an integrated trail network throughout town.

c) Protect identified trail corridors, including the Long Trail and Catamount Trail during the subdivision review process.

Chapter 5. Power for the People

Energy Goals (p. 5-5)

Goal 5.C Reduce direct and indirect transportation demands.

Energy Objectives (p. 5-5)

Objective 5.4. To create opportunities for walking, cycling and other energy efficient alternatives to the automobile.

Implementation Strategies

a) Complete a recreation and pedestrian path network plan for Warren. Encourage through regulatory and non-regulatory methods, the donation or provision of path easements from developers to enable creation of paths. Seek similar easements from owners of lands not proposed for development.

b) Continue to support state and regional public transportation systems, including the valley transit system. Ensure continued service to Warren Village.

c) Implement recommendations in the 2004 Warren Village Pedestrian Enhancement Plan and improve pedestrian access in the Lincoln Peak/Sugarbush Village growth center.

d) Encourage employers to provide incentives to promote energy efficient commuting (e.g. ride sharing, bicycling, use of Valley transit).

Chapter 7. Getting Around*Transportation Goals (p. 7-10)*

Goal 7.A Maintain and improve a transportation system that is safe and efficient.

Goal 7.B Minimize transportation energy consumption and trips.

Goal 7.C Support regional efforts to provide and maintain systems that meet the needs of all segments of the population.

Transportation Objectives (p. 7-10)

Objective 7.1. To direct growth to specified centers served by the existing main road network and limit growth in the remainder of town.

Implementation Strategies

- a) Reinforce existing centers at Warren Village and Lincoln Peak/Sugarbush Village.
- b) Continue to promote the principal use of minor arterial highways, specifically Route 100, and major collectors, specifically the Sugarbush Access Road and the German Flats Road, for recreation-related traffic.
- f) Evaluate parking needs and opportunities in Warren Village and Sugarbush Village. Require the examination of underground or structured parking as part of any expansion at Lincoln Peak.

Objective 7.2. To manage roads and bridges to meet community-level demand and maintain rural character.

Implementation Strategies

- e) Work with the Vermont Agency of Transportation and legislature to ensure that all road and bridge construction shall balance capacity requirements with scenic impacts to ensure that rural residential roads and bridges are not over-built to urban standards.
- i) To improve traffic flows during peak ski periods, encourage travel demand management techniques such as but not limited to satellite parking, use of public transit, or other options.

Objective 7.3. Provide alternatives to the heavy reliance on individual automobiles.

Implementation Strategies

- a) Require provisions for bicycles on any new or improvements to Class 2 or 3 roads and bridges.
- b) Coordinate and develop a path system with sidewalks to insure an integrated pedestrian network.
- c) Develop an aggressive ride sharing program.
- d) Support Vermont Agency of Transportation's striping and signage efforts to establish a dedicated bicycle lane on Route 100.

- e) Encourage the dedication of easements to permanently protect pathways through the subdivision and site plan review process.
- f) Support the continued operation of the Mad Bus or its equivalent and help implement the Short Range Transit Plan for the Mad River Valley (2002).
- g) Maintain the gateway for public recreation and river access at the property adjacent to the Route 100 and Access Road bridge with specific attention to pedestrians and cyclists. (Riverside Park)

Objective 7.4. Coordinate with local, regional and state entities to plan for Warren's transportation needs in a comprehensive manner.

Chapter 8. Serving the Community

Recreational & Cultural Activities (p. 8-11)

While the recreation base of Warren rests upon skiing and ski--related facilities, there have been recent efforts to increase year-round tourism through cultural activities as well as soaring, golfing, tennis, horseback riding, on- and off-road biking, and numerous related indoor and outdoor recreational activities. Fishing, hiking, swimming, canoeing, and crosscountry skiing continue to be popular pursuits.

Community Services, Facilities and Utilities Goals (p. 8-19)

Goal 8.A The provision of an environmentally sound and cost efficient system of community services, facilities and utilities to meet present and future demands of Warren citizens and visitors.

Community Services, Facilities and Utilities Objectives (p. 8-19)

Objective 8.2. To preserve the small town quality of Warren Village by maintaining and strengthening the Village as the center for town government and services.

Implementation Strategies

- b) Investigate the possibilities for a Town Green.
- c) Investigate the possibilities for additional public parking and public facilities to support a diversity of uses.
- d) Identify and, where possible, provide services and facilities necessary to enable Warren Village to continue to function as the town center, including an expanded post office.
- g) Provide permanent, accessible public restrooms in Warren Village.

Objective 8.5. To maintain the land resource necessary to support recreation and public access to recreation areas.

Implementation Strategies

- a) Where appropriate, acquire interest (e.g., conservation easement) in important recreation resource areas. Continue to fund the Town's Conservation Reserve Fund for this purpose.

- b) Encourage the U.S. Forest Service's acquisition of additional lands in Town to be included in the Green Mountain National Forest and consider expanding the Proclamation Boundary.

Objective 8.6. To support private organizations working to meet the many different needs of Warren.

Implementation Strategies

- f) Encourage and support the efforts of the Friends of the Mad River and the Mad Path's efforts to create a continuous bike path.

Chapter 9. Making Ends Meet

Local Economy Goals (p. 9-10)

Goal 9.A Promotion of a diverse local economy characterized by varied employment and entrepreneurial opportunity.

Goal 9.B Ongoing support of those businesses and industries which contribute to Warren's rural character and high quality of life.

Goal 9.C Ongoing support for the economic viability of Sugarbush Resort and other businesses which contribute to the Valley's attractiveness as a resort destination.

Local Economy Objectives (p. 9-10)

Objective 9.2. To encourage economic activities that contribute to the preservation of Warren's rural character.

Implementation Strategies

- b) Support the development of recreation and cultural facilities which contribute to the Valley's attractiveness as a resort destination. Such encouragement can take the form of:
 - iv. Expanding existing walking and bicycling paths and trails and ensuring that future development is designed to accommodate pedestrian connections between properties.
- c) Implement strategies to maintain the economic viability of agriculture and forestry and provide economic incentives, including the State Current Use Program and purchase of development rights and easements, to landowners who keep large tracts of open space undeveloped.
- d) Implement the improvements set forth in the Warren Village Pedestrian Enhancements Plan 2004. Consider other improvements for improving pedestrian circulation, enhancing Village character and addressing parking and traffic problems.

Chapter 10. Land Use

Land Use Goals (p. 10-13)

Goal 10.A The preservation of the Town's historic settlement pattern, defined by compact villages surrounded by rural countryside.

Goal 10.B The regulation of land development in a manner which protects important natural and community resources including farm land, forest resources, important wildlife habitat and water quality while allowing for diverse land uses in appropriate locations.

Goal 10.C The development of a compact village core at the immediate base of Lincoln Peak and Sugarbush Village to serve as the Town's primary growth center.

Land Use Objectives (p. 10-13)

Objective 10.4. To reinforce existing villages and designated growth centers as the focus of cultural, economic and residential activities in the Town, in a manner that respects the unique character of those areas.

Implementation Strategies

a) Strengthen Warren Village's status as a Town center in order to promote its social, governmental, commercial and residential function in the community, while taking special care to protect the residential character and the quality of life enjoyed by Village residents. Also maintain a viable site in the Village for a Post Office. ...

b) Implement the objectives and strategies related to pedestrian enhancement, traffic management and parking improvements (see Chapter 7) and Community Facilities and Services (see Chapter 8).

Central Vermont Regional Plan – 2003

Overview

As its name implies, the region (CVR) lies at the geographic heart of the State. Accordingly, it embodies many of the most celebrated qualities of Vermont's culture and landscape, and serves as its political hub, as well. (page 1)

It was in the valleys where early settlement began where population, commerce, and infrastructure have historically been concentrated in compact hamlets, villages, and cities. (page 1)

One of 5 issues identified by a 1997 Commission to study region

3) Natural Beauty - We place a high value on the scenery and resources the landscape provides. (page 3)

CVRPC believes that the results of our quality of life discussions demonstrate our desire to live in a region that will be a model of health,

wealth, knowledge, beauty, culture, and community for many generations to come. (page 3)

The goals and objectives of each element of this Plan implement the following broad policies of the Central Vermont Regional Planning Commission:

CVRPC encourages planning that identifies, respects and preserves our important historic, natural, cultural, and recreational resources.

CVRPC seeks to maintain a healthy environment and to respect the region's historic settlement patterns. (page 6)

Land Use Element

Farmlands, forest lands, and lands containing mineral resources are vitally important to the economy and character of our Region. This Plan encourages the protection of resource production lands and the livelihoods of the people who use them by recognizing their benefits, promoting their products, and rethinking the attitudes, policies, and land use patterns which threaten their existence. (page 9)

Agricultural

The lure of our pastoral landscape yields substantial indirect benefits from tourists, as well.

Farming helps to define the Region's cultural identity and provides Central Vermont residents with open space, recreational opportunities, aesthetic pleasure, and a sense of place.

Farmlands provide a variety of environmental functions from which we all benefit. They provide wildlife habitat. They capture carbon dioxide, thereby maintaining air quality. They help protect the integrity and function of our flood plains and wetlands. They can help maintain water supplies through groundwater recharge. Farms, as they exist in Central Vermont, are part of, and contribute to, the natural systems that sustain life. (page 10)

Forest Land

Many recreational pursuits are dependent on, or enhanced by, forest land, as is the aesthetic quality of the Region.

Resource Protection

Within our Region's boundaries are many ecologically sensitive areas and resources which serve as symbols of our natural heritage and barometers of the Region's environmental health. (page 11)

The preservation of ecologically sensitive places is a goal of this Plan. Human use of such areas should be accomplished in a manner which protects their integrity and function. Resource protection lands include: natural and fragile areas, critical wildlife habitat, groundwater recharge areas, surface waters, wetlands, and scenic areas. (page 12)

The Region's lakes, ponds, rivers and streams represent an invaluable resource. They provide water for drinking, and domestic and industrial uses. They generate hydroelectric power. They dilute and assimilate various effluent. They provide recreational and aesthetic value for public use and enjoyment. (page 13)

Scenic Areas

Central Vermont is a place of celebrated natural beauty. Its scenic landscapes not only enrich lives and spirits, and attract new businesses and residents, they also provide the basic ingredient for one of the Region's most important industries - tourism. Each year thousands of visitors travel here to see the mountain vistas, pastoral scenes, fertile valleys, historic villages, Interstate 89 (which has received awards for its scenic corridor), remote back roads, and woodlands ablaze with autumn color. Thus, it is in best interest, both psychologically and economically, to preserve the best of Central Vermont's visual splendor. (page 14)

Development***Growth Centers***

Could help to:

preserve the character and aesthetic integrity of the region and thereby, the tourism economy it fosters. (page 17)

Desired growth center features include a compact form, minimal impact on natural resources and critical habitat, a mix of uses, available

infrastructure, conformance with local planning and zoning documents and local support for proposed development within the area. (page 17)

CVRPC Land Use Goals and Policies

Goal 1: To promote sound management, conservation and use of the Region's natural resources.

Policies:

1. When land development does occur on important resource lands it should be designed to minimize or compensate for its impact on productive use. Off-site mitigation may be appropriate in some instances. Clustering with permanent protection of open space is strongly recommended to preserve the land base and avoid resource fragmentation, while allowing for equity retrieval for individual landowners. The density of clustered uses should reflect the limitations and capabilities of the subdivided parcel. In addition, the development should not materially alter the overall land use patterns of the area, nor should it interfere with accepted farming, forestry practices, or resource production on adjacent lands. Vegetative buffers between uses may be appropriate in some instances.
2. CVRPC also encourages the use of non-regulatory techniques (including tax stabilization programs, voluntary conservation easements, and purchase of development rights or fee simple purchase through community land funds or trusts), creative development, municipal "overlay protection zones," and off-site mitigation to protect important resource lands. (The Commission will assist landowners or municipalities in the analysis, development or implementation of such measures upon request.)
3. CVRPC encourages the use of creative development techniques and standards (e.g. clustering and open space development), municipal "overlay protection zones," and non regulatory techniques to minimize the impact of new development on land uses and ecological functions described in the "Resource Protection" section of this Plan. The Commission will assist landowners, developers or municipalities in the analysis, development, or implementation of such measures, upon request.
4. Municipalities are encouraged to establish conservation commissions (under V.S.A. 24, Chapter 118) to assist in the identification, study, maintenance and protection of important natural resources. (page 19)

Goal 2: To enhance and support the viability of the Region's resource based industries.

Policies:

3.CVRPC encourages municipalities to identify locally significant agricultural and forest parcels and/or districts through locally and consensually developed land evaluation and site assessment programs (e.g. LESA and FLESA). Such identification can assist in establishing protection priorities and programs. (page 20)

6.New developments which encroach upon resource lands, and the occupants thereof, are encouraged to respect the rights of resource land owners to continue existing operations, and undertake appropriate expansions, according to accepted practices. (page 21)

Goal 3: To encourage the historic settlement pattern of compact village and urban centers separated by rural countryside while promoting development in economically viable locations. (page 21)

Policies:

1. New development should be planned so as to respect the historic settlement pattern of compact villages, neighborhoods, and urban centers separated by rural countryside. Accordingly, CVRPC: (page 21)

A. Endorses the concept of creating new villages to accommodate new growth.

B. Supports the appropriate expansion of existing settlements. (The existing settlements within Central Vermont are those areas currently served public water and/or sewer systems or characterized by higher densities of development. Existing settlements include the downtowns and cities, the villages and the myriad concentrated residential neighborhoods.

C. Encourages "cluster" or "open space" design for new residential and commercial developments, particularly those outside of existing settlements or planned growth areas and discourages the development of commercial and residential sprawl.

D. Encourages "in fill" development and adaptive reuse of buildings in existing settlements.

E. Supports and encourages revitalization efforts directed towards strengthening and improving villages and cities.

F. Recognizes that some environmental and development "trade-offs" will be necessary to achieve desired growth patterns.

G. Believes that land use restrictions should not unduly hinder self-employment for residents. Such opportunities may help reinforce traditional land use patterns through economic incentives.

H. Believes that land use plans should not unnecessarily infringe upon the landowner's ability to enjoy and profit from the investment and use of private property. (page 22)

Goal 4: To protect environmentally sensitive or unique areas.

Policies:

1. Natural and fragile areas identified in this Plan should receive protection from harmful uses.
2. Where natural and fragile areas occur on developable private lands and where their adequate protection would preclude any other reasonable use of those properties, acquisition in fee simple or less than fee simple is recommended.
3. Where a potentially harmful development or activity is proposed in proximity to a natural or fragile area, measures should be taken to ensure adequate protection.
4. CVRPC encourages the inclusion of natural and fragile areas information and mapping in local plans. (Municipalities should not be limited by the definitions and designations included here, as it is recognized that this Plan may not include all locally significant sites.)
5. It is the policy of CVRPC to encourage the maintenance of existing critical wildlife habitats. Communities are encouraged to identify locally important habitats.
8. It is the policy of CVRPC to encourage the preservation of wetlands so as to protect their function and productivity. Efforts (including consideration of site design options) should be made to mitigate against the possible adverse impacts of development on the Region's wetlands. (page 22)

Goal 5: To preserve the aesthetic quality of the Region (page 22)

1. Municipalities and developers are encouraged, through design and siting of structures, to make a concerted effort to preserve access to and enjoyment of scenic views for the public.
3. The scale and siting of new structures should be in keeping with the surrounding landscape and architecture; however, towers should utilize stealth technology.
8. CVRPC encourages the State and municipalities to maintain existing roadside views by means of vegetation clearing, where appropriate.
9. CVRPC will attempt to inventory and map the Region's scenic resources, with assistance from municipalities. (page 23)

Transportation Element

Vision - "To maintain and develop a transportation system that facilitates

travel while preserving the Region's character."

Mission - "Preserve, enhance, and develop an integrated, multi modal regional transportation system to accommodate the need for movement of people and commerce in a safe, cost-effective, environmentally responsible, and equitable manner, that conforms with other elements of the Regional Plan."

GOAL 1. To achieve a regional transportation planning process that is comprehensive, multi-modal, and public, and is integrated with regional and local land use planning as outlined in the Central Vermont Regional Plan.

Policies:

1. Encourage municipalities' analysis of transportation needs at the local level, including the relationships between development patterns and transportation needs, and which considers various modes of travel.
2. Encourage coordination and cooperation in comprehensive transportation planning among the various municipalities in the Region and at the regional, State, and private levels.
3. Undertake a comprehensive regional analysis of existing and anticipated travel behavior and multi-modal approaches to accommodating anticipated travel demand.
4. Balance regional and local decision-making, and flexibility in transportation planning, when conflicts develop between local and State plans.
5. Promote a project prioritization process that takes the goals of the Regional Transportation Plan into consideration.
6. Promote open and inclusive public participation in the multi-modal planning and development of transportation projects.
7. Support the planning and design of the region's transportation system to encourage development and re-development in existing villages, cities, and designated growth centers.
8. Encourage the full integration of transportation and land use planning at the regional and local level. (page 42)

GOAL 2. To preserve and maintain the existing transportation system.

Policies:

1. Support the necessary steps for evaluating, prioritizing, and implementing preventive maintenance programs for all elements of the transportation system.
2. Promote a funding strategy that realizes maximum use of all available resources to ensure adequate maintenance of the existing transportation system.
3. Encourage development patterns that reflect the planned capacity of the

transportation system. Level of Service C will be taken as the preferred condition.

Level of Service D should be accepted within the more urban, built-up sectors of the region. (page 42)

GOAL 3. Enable the transportation system to operate at it's highest efficiency by managing travel demand and encouraging shifts to under-utilized and more efficient travel modes. (page 42)

Policies:

1. Develop a strategy that encourages maximum use of all available transportation resources and allocates those resources to the optimum functioning of the transportation system.
3. Educate the public on modal choices available.
4. Encourage preservation of existing rights-of-way for future transportation purposes. In particular, work to retain abandoned railroad rights-of-way for transportation uses such as trails and bike paths.
7. Establish aggressive, but realistic, targets for modal shares along regional transportation arteries.
9. Market public transit to new users. (page 43)

GOAL 4. To integrate modes of travel in order to allow for their most effective use and ultimately reduce dependence on single occupant vehicles.

Policies:

1. Encourage the development of park and ride lots for car and van pools, and encourage employers to provide incentives to car and van pool users.
2. Promote physical and operational connections between various modes of transportation.
3. Ensure adequate mobility for all segments of the population, including residents who cannot or do not use private automobiles.
4. Foster a sense of mutual respect among users of the various modes of transportation.
5. Encourage the availability of multiple options for the movement of people and goods. (page 43)

GOAL 5. To establish a transportation system that minimizes consumption of resources and maximizes the protection of the environment. (page 43)

Policies:

1. Support efforts to minimize negative environmental impacts associated with the transportation system (including air quality, noise levels, surface water, vegetation, agricultural land, fragile areas, and

historical/archaeological sites).

2. Encourage the preservation and enhancement of scenic views and corridors.
4. Factor direct and indirect costs and benefits into decision-making. Impacts that are not easily expressed in dollar values should also be considered.
5. Promote public awareness of the environmental impacts resulting from use of the region's transportation system.
6. Promote a transportation system that encourages concentrated development, allows greater access to residences, employment, and services, and facilitates car-pooling, bus and rail service, and non-motorized travel. (page 44)

GOAL 6. To make necessary improvements to achieve a transportation system appropriately structured and designed to safely, effectively, and economically move goods and people.

Policies:

1. Encourage the appropriate scale and design of streets, highways, and other transportation infrastructure to serve local traffic, destination traffic, and through traffic.
2. Foster a neighborhood street system characterized by a network of interconnected streets that minimizes through traffic in residential neighborhoods.
3. Promote safety-targeted measures at High or Potential Accident Locations, and promote traffic safety region-wide.
4. Promote projects that limit the conflicts between the motor vehicle traffic stream, pedestrians, and the rail system.
5. Encourage access management policies that reduce traffic congestion and maintain capital investment.
6. Consider new facilities when demand warrants (e.g. when alternatives to reduce congestion and improve safety have been attempted) and/or when other strategic state, regional, or local goals apply.
7. Foster a sense of safety and comfort for riders of public transit. (page 44)

GOAL 7. Promote a transportation system design that strives for aesthetic and functional characteristics that improve the quality of life. (page 44)

Policies:

1. Support the design of visually attractive and durable infrastructure such as roadways, pathways, and bridges.
2. Support high architectural standards for terminal buildings, stations, shelters, garages, and other facilities.
3. Respect and enhance the built environment by restoration of period

transportation structures where possible, and maintain the natural environment through architectural, landscaped, and engineered features.

4. Encourage traffic calming efforts to minimize conflicts between traffic and surrounding neighborhoods.
5. When feasible, encourage restoration or preservation of historic bridges.
6. Foster improvements that are contextually appropriate. (page 45)

GOAL 8. To promote a regional transportation system that preserves and enhances residential and economic development potential in growth areas.

Policies :

1. Provide transportation system improvements at locations where they will or can serve growth areas.
2. Foster transportation and commerce links that contribute to the economic health of the region.
3. Encourage transportation system improvements that renew and improve downtowns, growth areas, and neighborhoods. (page 45)

GOAL 9. To promote a regional public transportation system.

Policies :

1. Provide for basic mobility for transit-dependent persons.
2. Support public transit that provides access to employment.
3. Encourage congestion mitigation to preserve air quality and the sustainability of the highway network.
4. Support public transit that advances economic development with emphasis directed toward tourist areas. (page 45)

Utilities, Facilities and Services Goals and Policies

OUTDOOR RECREATION GOAL: To promote adequate access to a wide range of high quality outdoor recreation experiences to all sectors of the population. (page 75)

Policies:

1. CVRPC will encourage and foster the provision of diverse outdoor recreational opportunities, with consideration given to the needs of the elderly, disabled, and economically disadvantaged. (page 75)
2. CVRPC encourages, in particular, those recreation activities which focus on, respect, enhance, and educate, about the natural environment.
3. Recreation inventories and needs assessments should occur at the local and regional levels in order to determine deficiencies and conflicts, and to identify key recreational resources and opportunities on both public and private land.

4. Municipalities should develop and implement strategies to protect important recreation lands. Actions such as securing voluntary easements, fee or less than fee acquisition, subdivision or zoning regulations which contain provisions for common open space, impact fees or other contractual arrangements are encouraged as alternatives for achieving permanent or semi- permanent protection.

5. Public access to rivers, streams, lakes, ponds and recreation lands is a need in the Region.

Municipalities, the State, and private groups, such as land trusts, should coordinate efforts to provide for improved access to the Region's surface waters. At the same time, significant water related natural areas should be maintained and protected.

6. Priority consideration should be given to rehabilitating and upgrading existing recreation facilities.

7. CVRPC supports the maintenance or upgrading of existing surface water classifications to reflect their actual recreational uses, except where lower classifications may be needed for municipal sewage treatment projects.

8. Landowners are encouraged to voluntarily keep their lands open for public recreation and enjoyment where possible, so as to maintain the Region's tradition of informal, resource based recreation on private lands.

9. CVRPC will support future legislation to alleviate landowners of unreasonable liability burdens.

10. New development proposals are encouraged, through design, to make an effort to preserve access to recreational uses for the general public.

12. CVRPC will work towards and support the maintenance and development of trail and greenway networks to provide for recreational diversity, tourist amenity, habitat linkage, and low impact transportation choices. Specifically, the Commission will strive to:

- work with individual municipalities, at their request, to help plan local trails and green-ways;
- work with groups of municipalities and/or citizens to promote the concept and realization of a regional trail and greenway network which connects and builds upon local initiatives; (page 76)
- encourage the paving of shoulder for safe bicycle and pedestrian travel on all state highways in the region;
- encourage the development of multi-purpose trail corridors along abandoned rail beds;
- encourage municipalities to retain Class IV roads and public trails for public recreational use; and
- encourage the provision of recreation along utility corridors, as appropriate.

CULTURAL RESOURCES GOAL: To promote adequate access to a wide range of high quality cultural experiences for all sectors of the population.

Policies:

1. CVRPC encourages the development of new cultural facilities and services (including studio space), in Central Vermont, particularly in or near existing settlements and growth centers, as such areas are most accessible to all segments of the population, and the proliferation of culture in such areas will strengthen their vitality.
2. The protection and preservation of existing cultural resources and activities is a goal of the Commission.
4. The Commission encourages the rehabilitation or adaptive use of sites and structures for cultural pursuits. (page 77)

HISTORICAL AND ARCHEOLOGICAL RESOURCES GOAL: To promote the protection and use of the Region's historical and archeological resources.

Policies:

1. Municipalities are encouraged to provide a historic preservation section in their municipal plans. (CVRPC will assist in such an effort, if requested.)
2. CVRPC encourages development which preserves the historic and architectural character of town and village centers and the rural landscape.
3. Therefore, it is the policy of this Commission to support and encourage downtown revitalization programs. Downtown revitalization efforts are means to create jobs and to preserve our national heritage.
4. CVRPC encourages the restoration, rehabilitation and adaption of historic structures where feasible, as this minimizes the environmental impact of development by conserving raw materials, using land already developed, employing existing services.
7. The impact upon the historic character of the area should be considered when public or private municipal improvement projects (such as sidewalks, roads and traffic improvements) are proposed.
8. Activities having substantial impact on an important historical site or structure should be planned in consultation with the Division for Historic Preservation, Agency of Commerce and Community Development.
11. Prehistoric and historic archeological sites are recognized as important to Vermont's history. Any activity that may have an impact on a prehistoric or archeological site should be planned in consultation with the Division for Historic Preservation, Agency of Commerce and Community Development.

12. CVRPC will provide support to local, regional, and state non-profit historic preservation trusts upon request. (page 78)

13. CVRPC will promote the awareness of historic preservation through periodic publication of funding sources available to municipalities and investment tax credits available to individuals. (page 79)

Photo Inventory

Recreational



Moss Glen Falls viewing deck



Entrance to Moss Glen Falls from pull-off



Accessible walkway from pull-off to Moss Glen Falls viewing deck



Typical pull-off that is located along the entire corridor



Picnic table located at the Granville Gulf Rest Area



Picnic area located at the north end of Route 100 in Waitsfield



Signs point the way for VAST snowmobile access



Green Mountain National Forest Information Board, found at trailheads throughout the corridor



Long Trail signs inform hikers in Fayston



Entrance to Riverside Park



Sign for Punch Bowl Swimming Hole



Mad River Greenway trailhead- one of many paths maintained by the Mad River Path Association



Lareau rest area



Swimming hole located at the Lareau Rest area



Sugarbush Mount Ellen area



Glider rides offer a wonderful view of the corridor



Stables are located throughout the corridor, and offer some of Vermont's best trail rides



Fishing in the Mad River- one of the earliest summer activities in the corridor



The Mad River has been host to locals and tourists looking for a nice place to relax



Swimming holes such as this one have been summer gathering places for generations



Route 100 has much to offer a cyclist, with great views and multiple access points



Mush! Annual dog sled races have always drawn crowds



Recreation history was made in the Mad River Valley with the introduction of the ski industry



Mad River Glen in the summertime continues to provide recreational opportunities.



The Long Trail where it intersects at the peak of Route 17



The top of the Mad River Glen chairlift on Route 17



Trails throughout the valley provide easy access to the Valley's natural resources



Swimming holes are a popular place for summer activities

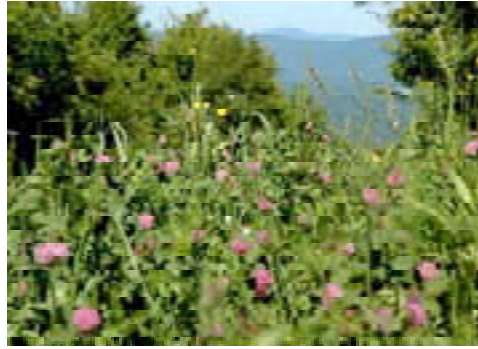


Recreational opportunities abound for users of all ages

Natural



The Granville Gulf Reservation protects acres of diverse forest



The corridor is home to many species of plant and animals



Large deciduous trees add beauty to the corridor



A stand of evergreens located in the Granville Gulf Reservation



An example of the ledges present at the Granville Gulf Reservation



The beautiful Moss Glen Falls, one of several waterfalls in the corridor



Large trees, shrubs, moss and ledge all interact and create natural beauty along the corridor



Tree stumps dot the landscape and remind residents and visitors of the past floods



Gorges display the beauty and strength of water. They can be found on the Mad River and many of its tributaries



Nature can be found at all points in the corridor, even where it is unexpected like next to this bridge

Scenic



View from the south into the Granville Gulf Reservation



The corridor's natural resources create picturesque scenic views



View of Mt. Ellen from Route 17-part of the Sugarbush Ski Resort



Foreground views of classic Vermont farmsteads stand in the shadows of Sugarbush Ski Resort



Sugarbush Ski trails as seen from the Village Covered Bridge in Waitsfield



Farm structures sit in the shadows as sun beams break through to the higher hills



Long distance views of the mountain ridges are visible at every twist and turn of the road



A mountain stream in fall, located at the entrance to Moss Glen Falls



Barns and other historic structures add architectural character to natural and pastoral views



Sweeping panoramic views are typical of the byway corridor



Stately pines and other tree species add interest to the views along Route 17

Cultural



Memorial highway sign



Historic dam in Warren currently used for private power generation



The Joslin Library located in Waitsfield



Artisan's Gallery, featuring Vermont crafts, located in Waitsfield



General Wait House and Visitor Center, located in Waitsfield Village



Waitsfield Elementary School- one of three schools located in the corridor

Historic



A horse drawn snow wheel used to pack snow on Route 100



Construction equipment used on what was soon to become Route 17



Historic millyard that is still in use today- located at the intersection of Route 100 and Route 17



Historic picture of a man and his logging truck



Historic millyard on Route 17



Historic Main Street in Warren



Gathering of townspeople in Warren Village to listen to a speech on Memorial Day 1890



Cars try to make their way on a mud and rut covered Route 17



Hikers on top of Mt. Abraham along the Long Trail near Lincoln and Warren, August 1920



Adjacent to the corridor, there are many historic covered bridges that are still in use today, including the Great Eddy Covered Bridge in Waitsfield and the Warren Covered Bridge



Maple sugaring has a long history in the Mad River Valley



Mad River Glen- "Ski it if you can"



Skiers enjoying the sunshine of a spring day outside the Sugarbush main lodge



The Glades- skiing at Sugarbush South



Three painted slides of the first road (Route 100) through the Granville Gulf Reservation



Early 1900s view of Route 100 entering Warren Village



Historic Main Street in Warren



Gathering of townspeople in Warren Village to listen to a speech on Memorial Day 1890

Byway Information Center



This property, the old Top Gas station located in Warren, has the potential to be used as a Byway Information Center- distributing recreational and scenic information as well as local amenities



CENTRAL VERMONT REGIONAL PLANNING COMMISSION



Jill Michaels, Chair
Scenery Preservation Council
PO Box 298
South Strafford, VT 05070

9/1/03

Dear Jill,

On behalf of the Towns of Morristown, Waitsfield, Fayston, Warren, Granville, and Buck's Grove, I am submitting a nomination package for the designation of Vermont Route 100 south of the intersection with VT 100 B to the southern end of the Granville Golf Reservation, and Vermont Route 17 from the intersection with VT 100 to the top of the Appalachian Gap as Vermont Byways (approximately 28 miles). The Central Vermont Regional Planning Commission supports this proposal because it meets the goals and policies of the Regional and Town Plans. The Central Vermont Regional Planning Commission also recognizes that designation as a Byway should not have special influence in the regulatory review process.

As the Corridor Management Plan identifies, these roads contain qualities of each of the six intrinsic resources defined in the Vermont Byway Program. Throughout the planning process, there has been strong community support. Designation will offer the Towns and the Region opportunities to improve the resources of the corridor. Please contact me if you have any questions or need more information.

Sincerely,

A handwritten signature in dark ink, appearing to read "Steve Gladezok".

Steve Gladezok
Transportation Planner

TOWN OF FAYSTON
888 NORTH FAYSTON ROAD
NORTH FAYSTON, VT 05660

August 16, 2007

Linda Lloyd
Mad River Valley Planning District
General Mail House
Weitsfield, Vermont 05673

Re: Vermont Byway Designation

Dear Linda,

At the Fayston Selectboard meeting on August 13, 2007 the Selectboard passed a motion to support a Vermont byway designation for the Fayston portion of Route 17.

If there is anything further you need please let us know.

Sincerely,

A handwritten signature in black ink, appearing to read "Robert Vassour". The signature is fluid and cursive, with a large initial "R" and "V".

Fayston Selectboard
Robert Vassour, Chair
Jared Cadwell
Edward Reed



TOWN OF WAITSFIELD

August 16, 2007

Steve Gladezduk, Transportation Planner
Central Vermont Regional Planning Commission
79 Main Street, Suite 4
Montpelier, VT 05602

Re: Mad River Valley Byway Designation

Dear Steve:

At its meeting on August 13, 2007, the Waitsfield Selectboard expressed strong support for participating in the Mad River Byway Program. Vermont Routes 100 and 17 are major thoroughfares through the center of the town that not only bring travelers into the community to enjoy commerce and the Valley's many splendors, they are vital corridors for Waitsfield residents and businesses in the conduct of their daily affairs. It is very important that we provide safe infrastructure for vehicles, pedestrians, bicyclists, and others, while at the same time protect the visual and environmental qualities that contribute the area's beauty, economic vitality, historic and natural resources, and rural integrity.

Thank you to you, the CVRPC, the Mad River Path Association, and the Mad River Valley Planning district for all of your efforts.

Sincerely,

Valerie Capels
Waitsfield Town Administrator

TOWN OF WARREN

**Municipal Building
P.O. Box 337
Warren, Vermont
05674-0337**

Office of:

Town Clerk
Town Treasurer
Assessor-Listers
Collector of Delinquent Taxes
Administrative Assistant
Zoning Administrator
Selectmen

Hours:

9:00-4:30 p.m.
Monday-Friday
Telephone: 802-496-2709
Fax: 802-496-2416

August 27, 2007

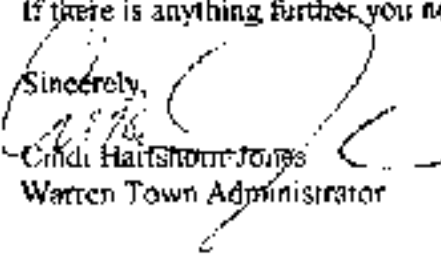
Steve Gladczak
Transportation Planner
Central Vermont Regional Planning Commission
29 Main Street, Suite 4
Montpelier, VT 05602

Re: Vermont Byway Designation

At the Warren Selectboard meeting on August 14, 2007, the Warren Selectboard passed a motion to support a Vermont Byway Designation for the Warren portion of Route 100.

If there is anything further you need, please do not hesitate to call

Sincerely,



Candi Hartshorn Jones
Warren Town Administrator

TOWN OF GRANVILLE

TOWN OFFICE

P.O. Box 66
Granville, Vermont 05747
802/767-4403

August 7, 2007

Steve Gladezuk
Central Vermont Regional Planning Commission
29 Main St. - Suite 4
Montpelier, VT 05602

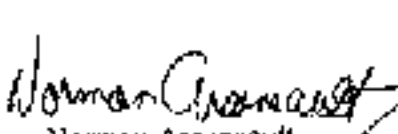
Dear Steve:

Thank you for attending the Granville Selectboard meeting last evening to bring us up to date on the proposal for a Mad River Byway along State Route 100.

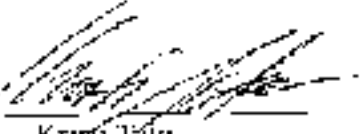
We understand that the Town of Granville would be the southern terminus of the Byway and that lands within the Granville Gulf Reservation would be the sole ownerships to be designated. Obviously, the Granville Gulf is a very scenic and naturally-appearing segment of Route 100, and it certainly fits in with the overall objectives of the Byway.

The Granville Selectboard would like to go on record as endorsing the Mad River Byway within our town. We understand the process has quite a ways to go yet with further hearings at several levels. Please keep us informed as the proposal moves along.

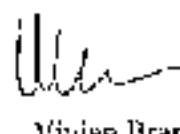
Sincerely,



Norman Arseneault
Selectboard Chairman



Kristi Dale
Selectperson



Vivian Branschofsky
Selectperson



RCM :

FAX NO. : 4983645

Jul. 19 2007 09:15AM 12

Town of Moretown
Office of the SELECTBOARD
P. O. Box 666
Moretown, Vt. 05660
802-498-3645
mselectboard@yahoo.com

July 19, 2007

VT Scenery Preservation Council
c/o Steve Gladezik, Transportation Planner
Central Vermont Planning Commission
29 Main Street, Suite 4
Montpelier, Vt. 05602

Dear Steve:

On behalf of the Moretown Selectboard, I am writing to express the support to designate Route 100 as a scenic byway.

This project affords the Town of Moretown opportunities to promote its efforts for preserving the beauty and rural nature of one of the most beautiful areas in Vermont. It is our further goal to create opportunities for our recreational, historical and scenic resources. Additionally, the Town wishes to promote the safety of all residents including those who enjoy this area as bicyclists, walkers and motorists.

Please contact me if you need further assistance to promote this project. Thank you for all the work you have done.

Best regards,

Don Wexler,
Selectman, Town of Moretown

Garret Mott
Supervisor, Ruel's Gore
129 Old County Road
Ruel's Gore, VT 05487

Friday, August 31, 2007

Steve Gladozuk
Transportation Planner
Central Vermont Regional Planning Commission
29 Main St. Suite 4
Montpelier, VT 05602

Dear Steve:

Ruel's Gore has been asked to write a letter in support of the Route 17 Byway. I, as Gore Supervisor, brought up the Byway designation at a Gore meeting last fall. The residents at the meeting were not enthusiastic about it.

Ruel's Gore, having no businesses on Route 17, has little to gain by this designation, while residents felt that we had much to lose. The main concerns voiced were the anticipated consequences of the designation: more traffic, causing safety issues for residents, more accidents, causing increased work for law enforcement and emergency responders, and more signage cluttering up the road, caused by both the above

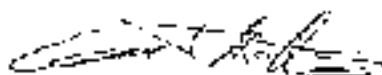
However, all understood that the Byway designation would be helpful for the towns in the Mad River Valley.

Therefore, Ruel's Gore will endorse the designation, with reservations. These are:

- 1) That printed materials about the Route 17 portion of the Byway mention the dangers of the road (particularly winter travel and the dangers for motorcyclists)
- 2) That any kiosk placed at the top of the Appalachian Gap also mention them
- 3) That there not be any increase in signage along the Gore portion of Route 17 ROW (except one or 2 Byway signs).

A further suggestion was made that a placing a list of the people who have died as a result of accidents on Route 17 on a kiosk at the top of the gap would help people to treat the road with respect. Listing whether the person was on/in a motorcycle or car might also help, as many who have died were riding motorcycles at ridiculous speeds.

Sincerely,



Garret Mott

Cc: Daniel Senechal-Albracht, Chittenden County Regional Planning Commission

MAD RIVER VALLEY PLANNING DISTRICT

August 16, 2007

Steve Gladczuk
Central Vermont Regional Planning Commission
29 Main Street
Montpelier, VT

Re: Vermont Byways designation

Dear Steve,

Please be advised that the Mad River Valley Planning District wholeheartedly supports the designation of Route 100 and Route 17 through the Towns of Warren and Waitsfield and Fayston as a Vermont Byway.

The Planning District assists all three towns with planning and is delighted to support the Byways effort.

We shall look forward to working with you on the Byway Management Plan and future projects to benefit the Valley and our Byway travelers.

Sincerely,


Linda Lloyd
Executive Director

*Mad River Path Association
P. O. Box 683
Waitsfield, Vermont 05673*

August 20, 2007

Mr. Steve Gladezuk
Transportation Planner
Central Vermont Regional Planning Commission
29 Main Street, Suite 4
Montpelier, Vermont 05602

Dear Steve,

I am writing this letter in support of the Mad River Byways project

As Executive Director of the Mad River Path Association, I have been working with the Moretown Byway group and am looking forward to continuing the Byway through the Mad River Valley and on to Granville.

Our organization has constructed and is maintaining over 13 miles of recreation trails in Warren, Waitsfield and Fayston. We are supported by the Recreation District, the Mad River Valley Planning District, the Chamber of Commerce and our many members who make an annual donation to help us with our mission of creating a continual recreation path that runs the length and breadth of the Mad River Valley. I feel that the presence of the Mad River Byway will enhance our mission, allowing us to become eligible for grants that will help us build and maintain recreational trails that are available for use at no charge for our local residents and out-of-town visitors.

Good luck with the Byways Designation.

Sincerely,

Carol Thompson

Carol Thompson
Executive Director
Mad River Path Association



August 17, 2007

Steve Gladczuk
Transportation Planner
Central Vermont Regional Planning Commission
29 Main St. Suite 4
Montpelier, VT 05602

Dear Steve:

Please accept this letter of enthusiastic support by the Mad River Valley Chamber of Commerce for a Scenic Byways designation for Rt 100 Moretown to Franville corridor and adjacent Rt 17.

The funds that would be available to enhance the corridor as a tourist destination is very valuable and we look forward to continuing to offer our support and ideas as these enhancements discussions and resulting implementation commence.

Preserving, enhancing, and promoting our natural assets, including the historic, natural, cultural and recreational is very important to the continued economic health of our community.

Sincerely,

Susan M. Roy
Executive Director

Discover Our Good Nature.

Chamber of Commerce • P.O. Box 173 • Wadswold, VT 05673 • Ph: 1 800 82 V1S1T • Fax: 802 556.5420 • Email: info@madrivervalley.com

www.madrivervalley.com



TWO RIVERS-OTTAUQUECHEE

William B. Fennous, III, Chairman
Peter G. Gregory, AICP, Executive Director

REGIONAL COMMISSION

August 22, 2007

Jill Michaels, Chair
Vermont Scenery Preservation Council
3117 Rose Hill
Woodstock, Vermont 05091

RE: Mad River Byway - Granville

Dear Jill:

The Two Rivers-Ottawquechee Regional Commission supports the designation request of the Mad River Byway for that segment that lies within the Granville Gulf area on Vermont Route 100. That area of our region is exceptionally scenic with much natural beauty and is worthy of designation under the criteria found in the Vermont Byway Program Manual.

Please let me know if you need additional information.

Thank you.

Sincerely,

Peter G. Gregory, AICP
Executive Director

cc: Town of Granville
Steve Gileczuk, CVRPC

3117 Rose Hill, The King Farm, Woodstock, VT 05091
802.457.3138, fax: 802.457.4728, www.trorc.org

Barnard • Bethel • Bradford • Benning • Bridgewater • Brookfield • Chelsea • Corinth • Fairlee • Granville • Hancock • Hartford • Hardland
Newbury • Norwich • Putney • Plymouth • Panton • Randolph • Rochester • Royalton • Sharon • Stockbridge • Stafford • Thetford
Topsham • Tunbridge • Vergennes • West Fairlee • Woodstock

September 10, 2007

Steve Gladezyk
Transportation Planner
Central Vermont Regional Planning Commission
29 Main Street, Suite 4
Montpelier, VT 05602

Dear Steve:

We have received a copy of the August 31, 2007 letter from the Buel's Gore Supervisor, Garet Mott to you. As you know, the letter endorses State Byway designation of the Route 17 Byway including the approximately 1,240 feet of Route 17 in the Gore from the Fayston Town Line to the top of the Appalachian Gap.

Although Buel's Gore has little to gain by this designation, both the Gore and the CCRPC understand that it will benefit the towns of the Mad River Valley. We are also in agreement however that travelers be made aware of the dangers of traveling on this road and that this information be made clear in the Byway's educational and promotional materials and that there be no increase in signage in the Gore portion of Route 17 except one or two Byway trailblazer signs. The CCRPC would also like to request that the Mad River Byway and the CVRPC monitor traffic conditions and volume along Route 17 so that appropriate measures can be maintained to prevent any worsening in traveler safety.

The CCRPC endorses the designation in concert with the concerns identified above. Please keep the CCRPC and Buel's Gore informed of the Byway's progress, projects and plans.

Sincerely,



Gregory Brown
Executive Director



State of Vermont
Operations Division District 6
186 Industrial Lane
Barre, Vermont 05641
ernie.englehardt@state.vt.us

Agency of Transportation

[phone] 802-828-2691
[fax] 802-828-3530
[toll] 800-253-0191

September 10, 2007

Steve Gladitzki, Transportation Planner
Central Vermont Regional Planning Commission
29 Main Street, Suite 4
Mantouche, Vermont 05602

Re: Vermont Byways Program - Vermont Routes 100 and 17


Dear Steve:

I'm writing in support of the proposal to establish Vermont Route 100 from its intersection with Vermont 100B in Moretown south through the Granville Gull Reservation, and Vermont Route 17 from Irasburg up to Huel's Gore, as a Vermont byway. This seems to be a natural and logical extension of the recently established Mad River Byway on Vermont Route 100B from Middlesex Village down to the Vermont 100-Vermont 100B intersection in Moretown.

The appeal of the natural, historic, recreational and cultural resources along this roadway network and corresponding corridor continues to attract State residents and visitors. Within the past three years, three national bicycle and running events have become established as annual events.

Comprehensive management of the corridor, as outlined in the Mad River Byway Extension Transportation Management Plan, will play a vital part in providing an efficient, useful and safe transportation system for all who travel through or use this corridor.

Sincerely,


Ernest C. Englehardt, Jr., P.E.
District Transportation Administrator

EGE/dm



Steve Gladczuk

From: Maciejowski, Jay [Jay.Maciejowski@state.vt.us]
Sent: Thursday, August 23, 2007 3:00 PM
To: Steve Gladczuk
Cc: Frayser, Mike
Subject: RE: Mad River Byway

Steve: I spoke to our Director of State Lands about your request for a letter of support from ANH. I am putting together a letter for someone at a higher level than me to sign and am hopeful that we could get something to you by the end of the month. - Jay

Jay Maciejowski
District Forestry Manager
Vermont Agency of Natural Resources
Department of Forests, Parks and Recreation
100 Mineral Street, Suite 304
Springfield, VT 05156-3188
Telephone: 802-885-8820
FAX: 802-885-8890
jay.maciejowski@state.vt.us
www.vtfrp.org

From: Steve Gladczuk (mailto:gladczuk@cvregion.com)
Sent: Wednesday, August 15, 2007 12:17 PM
To: ltopfsover@ts.fed.us; Maciejowski, Jay
Subject: FW: Mad River Byway

I have met with the Morstown, Waitfield, Fayston, Warren, and Granville Select Boards, and they are all supporting byway designation for VT 100 or 17 within their jurisdiction. I'm working on getting a support letter from Buels Cove and the GCRPC, TRORC, and CVRPC. Attached is the Granville Letter. Can I get a letter from ANH and G&NH?

We are trying to wrap up the Corridor Management Plan by the end of the month, get on the Scenery Preservation Council's September 10 Meeting Agenda, and schedule the site visit/public hearing before the leaves come down.

Steve Gladczuk
Transportation Planner
Central Vermont Regional Planning Commission
29 Main St. Suite 4
Montpelier, VT 05602

Phone - 802-229-0389
Fax - 802-223-1977
email - Gladczuk@cvregion.com
website - centralvtplanning.org